

APPENDIX F

Historical Resources Reviews

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To: Stephanie Strelow, Dudek

From: Bridget Maley, Principal, architecture + history, llc

Date: September 26, 2017

Introduction

At the request of Dudek, an environmental consulting firm, and the City of Santa Cruz (City), architecture + history, ll (a + h) has reviewed the proposed Master Plan for the Santa Cruz Wharf (Wharf) to understand if the plan would result in impacts to the historic resource present. The Wharf is listed in the City's Historic Building Survey and its importance in the City's historical development is discussed in the City's Historic Context Statement. This memorandum will inform the California Environmental Quality Act (CEQA)-required Environmental Impact Report (EIR).

Pursuant to the CEQA Guidelines, "historical resources include a resource listed in, or determined to be eligible for listing in the California Register of Historical Resources; a resource included in a local register of historical resources; and any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California."

A "substantial adverse change in the significance of an historical resource" means physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historical resource would be materially impaired. The significance of an historical resource is materially impaired when a project demolishes or materially alters in an adverse manner those physical characteristics of an historical resource that convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register of Historical Resources or local register of historical places.

The Santa Cruz Wharf is included in the City's Historic Building Survey (Volume 1) with a rating of "Excellent", which was based on an evaluation of the structure's historical



significance, architectural significance, importance to the neighborhood, alterations, and physical condition. The Wharf is described in the Survey as “a typical ocean pier, originally to accommodate shipping and now largely devoted to restaurants and pleasure fishing.” The Wharf is not designated as a City Landmark. None of the existing buildings on the Wharf are included in the City’s Historic Building Survey.

Methodology

Bridget Maley of **a + h** met with Stephanie Strelow of Dudek (previously of Strelow Consulting) at the Wharf structure for a site visit in December 2015 and to review the proposed Master Plan. Ms. Maley reviewed background information on the Wharf’s history and construction (listed below). Lastly, Ms. Maley and Ms. Strelow met with City of Santa Cruz Planner Norm Daly to review the Master Plan elements that could possibly impact the historic resource. Lastly, Ms. Maley reviewed the photographic simulations, dated September 2017, completed for the EIR that illustrate the new buildings to be constructed on the Wharf.

Documents Reviewed

Charles Hall Page & Associates, Inc. *Santa Cruz Historic Building Survey*. Volume 1. 1976.

City of Santa Cruz. Presentation to the California Coastal Commission regarding the Santa Cruz Wharf Improvement Project (Master Plan). October 2015.

Dunn, Geoffrey. *Images of America: Santa Cruz Wharf*. Charleston, SC. Arcadia Publishing, 2016.

Lehmann, Susan. *Historic Context Statement for the City of Santa Cruz*. Prepared for City of Santa Cruz Planning and Community Development Department. October 2000.

Leif Rideout, Architect, Photographic Simulations of Wharf Alterations, 8 views, existing and proposed, September 2017.

Roma Design Group. Wharf Master Plan. Prepared for the City of Santa Cruz. October 2014.

URS Corporation (architectural historian Corri Jimenez). California Department of Parks and Recreation (DPR Form Series 523) for the Santa Cruz Municipal Pier.



December 12, 2012.

Location, Background & Resource Description

The Santa Cruz Wharf is owned and operated by the City of Santa Cruz. It is a major visitor attraction featuring restaurants, fishing areas, fish markets, gift shops and other businesses. The Wharf is situated at the southern end of Pacific Avenue at Beach Street within the Beach Area of the City of Santa Cruz. The Wharf extends into Monterey Bay for a distance of approximately 3,000 feet; the initial approximately 200 feet of the Wharf spans the City's Main Beach. From shallow waters at the shore, the Wharf extends to water depths of 35 feet at its far end. The wood structure stands approximately 22-feet above mean sea level. The Santa Cruz Beach Boardwalk and the City's Main Beach are located to the east, and Cowell Beach and the Dream Inn are located to the west. A mix of visitor-serving and commercial uses is located along Beach Street to the north of the Wharf.

The Wharf is a timber pier, entirely constructed of wood and supported by approximately 4,445 Douglas fir piles. The Wharf is the longest timber pile-supported pier structure in the United States and one of the longest in the world. Historically called a wharf because of its early function in off-loading cargo, the Santa Cruz Wharf is actually a pier structure that extends to deep water, historically facilitating the mooring of large vessels, unlike a wharf which typically runs parallel to the shore.

The Wharf is supported by 12-inch diameter Douglas fir timber piles. Piles are driven in rows (bents) at approximately 15-foot centers, and spaced along the row. Piles are referenced by the location on the bent, numbered from the west and the bent number counting from the shore. For example, pile 3 bent 120 is the third pile from the west edge of the Wharf on the 120th row (bent) from the shore. The bents are spanned by 4 x 12 inch beams ("stringers"). Two inches of asphalt paving overlays the Wharf deck on roads and walkways.

The Wharf currently has approximately 60,000 square feet of commercial building space. These spaces are primarily occupied by restaurant and retail uses, approximately 40,000 square feet is leased to restaurants and 20,000 square feet to retail uses, almost all of which are located along 1,300 feet of frontage on the west side of the Wharf. Other buildings on the Wharf include a building for Wharf operations and a life guard station.



Today, the Wharf consists of approximately 7.5 acres of usable space, with approximately 50% of the Wharf accommodating vehicular circulation and parking; there are approximately 440 public parking spaces on the Wharf. The parking areas also include large enclosures for trash collection, Wharf equipment, rental boats, and a variety of other appurtenances.

Historical Overview

The Santa Cruz Wharf was constructed in 1914, as the last in a series of six piers that were constructed on the Santa Cruz waterfront between 1849 and 1914. The Wharf was originally envisioned as a commercial enterprise, built with public funds to further the economic development of the City. Soon after its construction, the Wharf became an attractive facility for the mooring and off-loading of commercial fishing vessels. Since its construction in 1914, the Wharf has expanded and widened from its original 100-foot width over the years, but the location and length of the Wharf remain unchanged. Since its construction in 1914, the Wharf has expanded from approximately 4.2 acres to 7.5 acres. The Wharf surface increased by 3.3 acres between the 1950s and the 1980s for commercial uses and parking.

Davits for lifting fishing vessels into and out of the ocean once lined the Municipal Wharf, as did a rail line, warehouses, and fishing-related storefronts. The rail line was taken out in the late 1940s, and into the early-1950s, and replaced with a paved deck for vehicle traffic.

Over the years, the Wharf has evolved in role, function and identity. From its initial role as a cargo handling and shipping pier to its later adaptation to serve the commercial fishing industry, the early decades of the Wharf were closely tied to the resources of Monterey Bay. After World War II and beginning in the 1950s, the Wharf was significantly expanded for commercial uses and parking. The commercial uses were initially a direct outgrowth of the commercial fishing industry, incorporating fish sales and featuring prepared seafood dishes in an open air setting in close conjunction with off-loading and handling of the daily catch.

Many of the Wharf's original buildings and structures have been demolished or greatly altered, including a large warehouse building that was located at the bayward end of the Wharf. According to the Wharf Master Plan, this structure, although now removed, was significant from a historic point of view because "it gave physical expression to the environmental conditions that made deep water maritime functions possible and that



contributed to the configuration of the end of the Wharf to optimize berthing relative to wind and wave conditions.” Today, the largely industrial and commercial oriented historic-era businesses of the Municipal Wharf have been replaced by restaurants, retail, and other tourist oriented services. Over 20 buildings and structures associated with these businesses are located on the deck of the Wharf, as well as other built environment features associated with utilities and lighting.

The *Historic Context Statement for the City of Santa Cruz* (Lehmann, 2000) indicates that within the context Santa Cruz’s economic development between 1850 and 1950, the Municipal Wharf represents an important property type related to industrial development and transportation. The wharves and piers first built in the early days of Santa Cruz’s history evolved with the changes in the City’s economy. The shipping wharves gave way to a railroad wharf and finally to a fishing and commercial wharf that was constructed in 1914 to serve the City’s needs. The Wharf has been rebuilt and refurbished over time, including a significant project in 1984, and the warehouses and commercial fishing boats have been replaced by restaurants and concessions for sports fishing and sightseeing. The circa 1920s fishing boat, the Marcella, is on display at the north wharf, and, as stated by Lehmann, may be the “last remnant of the time when the waterfront served the needs of a booming industrial economy.”

As the last of a series of six piers constructed to serve industrial and commercial development in Santa Cruz, and which have been a defining element of the Santa Cruz waterfront since 1849, the Santa Cruz Municipal Wharf serves as a vital physical reminder of that history. The historical fishing industry was also heavily dependent on the wharves of Santa Cruz, including the Municipal Wharf, prior to 1950.

The Wharf no longer serves the commercial fishing industry. Currently, the Wharf is one of a number of destination attractions in Santa Cruz’s Beach Area. The City estimates that approximately 2.5 million visitors currently come to the Wharf annually. Although the Wharf provides opportunities for pier fishing, as well as kayak and small fishing boat rentals, the Wharf’s identity is presently related to tourist-oriented commercial uses, including restaurants, small-scale retail operations, and souvenir shops running its length.



Project Description – Wharf Master Plan

The Wharf Master Plan proposes a number of immediate and longer term improvements for both the Wharf structure and associated amenities. Key components of the project that could possibly impact historic resources include the following:

- **New entrance sequence, gate and signage:** This includes moving the current entry gates and paid parking kiosks further south on the Wharf, design and development of a “gateway arch” sign that would serve as the visual and ceremonial entry on to the Wharf, and improve directional signage at the entrance to the Wharf complex.
- **Welcome center:** A new building housing an interpretive and welcome center would be developed at the northeast end of the Wharf. It would also include an open water swimming facility with access into the water and changing rooms.
- **East promenade:** This component of the Master Plan would result in expansion of the Wharf on the east side by 26-30 feet. This extension would consist of a hardwood deck supported by 12-inch timber piles. The expanded area would be constructed at a slightly higher elevation than the existing Wharf with a step-down section at the eastern edge to provide a place for sitting, fishing and viewing without interrupting visual access from the main deck level for those who are walking, strolling, jogging or bicycling. An approximate 18-inch tall seat wall would be located on the western edge along the parking side of the East Promenade to provide additional separation from the adjacent parked vehicles and an informal resting place.
- **East side small boat landing:** This improvement would be a small boat landing facility on the east side adjacent to the East Promenade. It would include facilities for kayak, paddleboard, and fishing boat rentals as well as Wharf operations. It is envisioned as a small structure.
- **Expanded Lifeguard Station:** The existing life guard station toward the north end of the Wharf would be expanded, possibly to include a second story. This would allow additional needed storage for rescue equipment, and to better office and meeting accommodations for the Lifeguard Headquarters.
- **West side events pavilion:** This would involve improvements to the already existing events area to create a more usable weatherproof area for a variety of events and activities.



- South landing: This would involve construction of a landing facility for the docking of larger vessels at the end of the Wharf for science, education, research, sports fishing and whale watching.
- New Landmark Building (south end of Wharf): This aspect of the Master Plan would create a new Landmark Building reminiscent in scale and form of the structure once located at the end of the Wharf. It would provide a visual focus and destination attraction to entice visitors to venture to the end of the Wharf.
- Stepped overlook at south end: This feature would heighten the visitor experience by creating a sensation of being out over water. It would create a Stepped Overlook that extends out into the Monterey Bay on the south end of the Wharf (near the Landmark Building).
- Improvement to western walkway: This component of the project involves construction of a 12-foot wide walkway on the western side of the Wharf that would provide for continuity of access along this side of the Wharf, which is currently lacking.
- Structural improvements: These would be on-going improvements as required by continuing maintenance and inspection of the facility.
- Utilities improvements: These would be on-going utility improvements as required by continuing maintenance and inspection of the facility.

Definition of the Historic Resource

The CEQA Guidelines (section 15064.5(a)(2)) indicates that a resource in a local register shall be presumed to be historically or culturally significant. Public agencies must treat any such resource as significant unless the preponderance of evidence demonstrates that it is not historically or culturally significant. Furthermore, the CEQA Guidelines indicate that generally, a resource shall be considered to be “historically significant” if the resource meets the criteria for listing on the California Register of Historical Resources, including the following:

- (1) Is associated with events that have made a significant contribution to the broad patterns of California’s history and cultural heritage;
- (2) Is associated with the lives of persons important in our past;



- (3) Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
- (4) Has yielded, or may be likely to yield, information important in prehistory or history.

A review of the historical significance of the Wharf was conducted in 2012 as part of studies prepared for the Regional Desalination Project. The review included preparation of a California Department of Parks and Recreation “Primary Record” (DPR) form. The historic resource evaluation concluded that the Wharf has historical significance at the local level due to listing in the City’s Historic Building Survey and also is eligible for listing in the California Register of Historical Resources (California Register) under Criterion 1 due to its association with the economic development of Santa Cruz and the long history and role of wharves along the Santa Cruz waterfront.

Evaluation for eligibility for the California Register requires establishment of historic significance and consideration of “integrity,” which refers to those features necessary to convey its significance. While a property’s significance relates to its role within a specific historic context, its integrity refers to “a property’s physical features and how they relate to its significance.” The California Register has identified seven aspects of integrity: location, setting, design, materials, workmanship, feeling and association.

While the materials of the Wharf have been changed over time, the current materials of both the Wharf substructure and those used on the more modern buildings situated on the Wharf are compatible in character with those used historically. The structure retains integrity of location and overall design, workmanship, feeling and association. The larger context or urban setting in the Beach area of Santa Cruz has evolved over time, becoming increasingly built up and urban, but this has not impaired the overall integrity or visual significance of the Wharf. The Santa Cruz Wharf has been continuously used since its original construction in 1914 and, although it has undergone numerous functional and structural changes since that time, it remains a vital part of the fabric of the Santa Cruz waterfront.

The Municipal Wharf is located in the place where it was originally built in 1914 at the base of Pacific Avenue, which historically provided a direct connection between the wharf and downtown Santa Cruz. The other piers and wharfs that once lined this part of the



Santa Cruz waterfront are no longer extant. In terms of design, the Municipal Wharf retains its original design intent to provide Santa Cruz with a direct connection to ocean industries and shipping, despite the fact that its function significantly changed after circa 1950. For example, commercial and recreational fishing operations shifted from the Wharf to the new Santa Cruz Harbor when it was completed in 1964. This historical connection is further enhanced by the setting on the Santa Cruz waterfront, and the wharf's prominence in views from West Cliff Drive and Beach Hill.

The Municipal Wharf helps convey the sense of place and orientation of Santa Cruz along the waterfront which was integral in the development of the City. It retains its integrity of feeling since it still retains its significant physical characteristics that convey its historic qualities, and evokes a sense of its historical past. It retains its integrity of association as the property is directly linked to past significant events, such as its use in shipping and the fishing industry. The majority of the businesses currently on the Municipal Wharf have been present for decades, including a few of the restaurants. These restaurants do not reflect the original fishing-related warehouses and buildings that historically occupied the wharf, but are indicative of the early Italian-American adoption of the Wharf as a place of business, as well as the shift from primary fishing to tourist enterprises by those early families.

The Wharf is the last of the original six wharf and piers that were constructed, and is the only surviving property associated with the shipping and fishing industries of the City from the early 20th century, which further elevates its significance. Although fewer than five percent of the original pilings still exist and there have been additions and alterations to the structure within the past fifty years, which has led to major losses in historic materials, it still conveys its historical significance, especially as the last surviving example of a property type pivotal in the historical development of the City.

The elements and integrity that make the Wharf potentially eligible for listing in the CRHR include its location, design (general placement of deck over piers, and distinct turn at end of pier), setting (e.g., proximity to railroad truss bridge and the Boardwalk), feeling (still "reads" as a large functioning pier), and association (the Wharf is still the place where economic and transportation activities took place that were important in the development of Santa Cruz), rather than specific materials and workmanship which were necessarily replaced and adapted over time to support the continued functioning of the pier



Assessment of Potential Impacts

Adoption and implementation of the proposed Wharf Master Plan would result in construction of new facilities and improvements that would alter the Wharf structure. As discussed above the Wharf is considered a historical resource under CEQA. According to CEQA (section 21084.1), a project that could “cause a substantial adverse change in the significance of an historical resource” may have a significant impact. CEQA Guidelines section 15064.5(b)(1) indicates that a “substantial adverse change in the significance of an historical resource” means “physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historical resource *would be materially impaired*.” Subsection (2) further indicates that the significance of a historical resource is *materially impaired* when a project “demolishes or materially alters in an adverse manner those physical characteristics of a historical resource that convey its historical significance” that justify its inclusion in or eligibility for listing in the CRHR or its inclusion in a local register.

The proposed Wharf improvements would be executed in similar materials as have been used historically on the Wharf and new structures or amenities proposed are of similar type, scale, massing and materials as those already in existence on the Wharf. Historically, there was a large, double-height interior warehouse at the south end of the Wharf, as evidenced in historic photographs reproduced in Geoffrey Dunn’s *Santa Cruz Wharf*, including on pages 63-67, 96, 101, 108. This building was removed in the late 1950s, but is the model and inspiration for the proposed Landmark Building (further discussed below).

The proposed new buildings and amenities on the Wharf would not impair the historic character, feeling or association of the Wharf and they would be designed to be compatible with the current and past elements along the Wharf. All of the proposed improvements appear to have been designed with the *Secretary of the Interior’s Standard for the Treatment of Historic Properties* in mind and the overall intent of the proposed project is to ensure the longevity of the Wharf both materially and economically.

While these improvements would likely increase uses along the Wharf, these uses would not impact the overall historic aesthetic of the Wharf. When considered collectively the suggested long-term maintenance, increased uses, improved infrastructure, and new building and amenities would not result in “substantial adverse change” in the significance of the Santa Cruz Municipal Wharf and therefore, the impact would be *less than significant*.



While many of these improvements would be considered alterations to the historic Wharf structure, they would ultimately provide for the long-term maintenance, care and use of the structure. The new or remodeled/infill buildings would potentially change or alter the historic Wharf in scale, massing and materials. However, none of the planned facilities and improvements envisioned in the proposed Master Plan would demolish, destroy, or relocate the Wharf such that it could no longer convey its historic significance. None of the proposed projects is of such a scale or intervention that the Wharf would no longer be considered an important visual feature of the Santa Cruz waterfront. The improvements would ultimately enhance the ability of the Wharf to continue to contribute the economic vitality and tourist industries of Santa Cruz. The Wharf structure would be expanded for the East Promenade and Westside Walkway, but the expansion would retain the same linear form as currently exists. The other improvements would not substantially alter the Wharf in a way that would diminish its historic significance.

The 2001 DPR form indicates that the eligibility and significance of the Wharf relate to its association with the economic development of Santa Cruz and long history of wharves along the Santa Cruz waterfront. The adoption of the Master Plan and subsequent construction of anticipated improvements would not alter the Wharf's association with the economic history of Santa Cruz nor would it change the Wharf's location. Future improvements would not adversely impact the physical characteristics that convey the historical significance of the Wharf as none of the improvements would not alter the overall historic integrity of the resource. The Wharf has evolved and changed over time as uses, needs, and users have changed. These incremental alterations and improvements have not resulted in any significant physical changes that impacted the historic character of the Wharf.

Additionally, the Master Plan proposes construction of a new "Landmark Building" at the end of the Wharf. The new building would be sited in the historic location of a large warehouse. According to the Master Plan, "the restoration of this building would re-establish a strong visual terminus to the Wharf" and "would provide an icon tied to its maritime traditions". The Master Plan indicates that this building and location "recalls the historic warehouse structure that once occupied this key location." The re-introduction of this feature near the southern terminus of the Wharf would be in keeping with the historic character and uses. The building would be designed to reflect the shape and form, but would not necessarily exactly mimic, the historic structure that once stood at this end



of the Wharf. The proposed structure, while not fully designed, appears to be generally compatible in its scale, massing, and materials to both the current and historic structures of the Wharf. The proposed new Landmark Building is heavily glazed at all sides and would be open at both the north and south ends. In conclusion, the construction of this new structure on the Wharf would not result in the “substantial adverse change” of the significance of the Wharf and would not visually alter the Wharf, such that it no longer conveys its significance.

The expansion of the Lifeguard Headquarters from a one- to two-story structure at the south end of the Wharf would not result in the “substantial adverse change” of the significance of the Wharf and would not visually alter the Wharf, such that it no longer conveys its significance.

The relocation and introduction of a new entry gate would result in physical alteration of the Wharf structure with a small expansion of the deck, which would be constructed of wood, and installation of 30 piles – 24 12-inch piles and six 14-inch steel piles. The alteration would not change the location or setting of the Wharf, and the 12-inch timber piles are the same as those that are currently used to replace damaged or worn piles. The scale and massing of the new entry gate would be compatible with the overall character of the Wharf. The change would not adversely affect the physical characteristics of the Wharf that convey its historic qualities and would not result in “substantial adverse change” in the significance of the historic resource.

The construction of the East Promenade would not demolish, destroy, or relocate significant elements of the Wharf. Expansion of the Wharf by approximately 1.5 acres to create the East Promenade would retain the same linear form of the Wharf as currently exists. The approximate 1.5-acre expansion would result in a pedestrian facility with a hardwood deck supported by approximately 525 new 12-inch timber piles. This linear addition to the Wharf would reflect the maritime character of the original Wharf structure. The materials used and the design employed would be consistent with the other incremental changes and features that have been added to the Wharf over time. The East Promenade improvements would not result in a significant visual change to the Wharf, nor would they result in the “substantial adverse change” in the significance of the Wharf as a historic resource.



The improvements proposed for the West side of the Wharf are mostly structural (new piles) and cosmetic (improvements to the facades of the existing buildings). These alterations would not change the overall character of the Wharf and would not impair the ability of the Wharf to convey its historic significance.

Conclusion

The Santa Cruz Wharf has an important place in the City's history and it is an identified historic resource. The proposed project would not result in cumulative changes that would impair the significance of the historic resource. Overall, the proposed changes are in concert with the recommendations found within *the Secretary of the Interior's Standards for the Treatment of Historic Properties*. The project employs materials and elements that would marry the older materials, buildings and components of the Wharf with the proposed new elements and interventions necessary for both the structural and economic longevity of the historic resource. None of the proposed components of the project would result in impaired historic integrity of the resource, nor would they rise to "substantial adverse change" in the significance of the historic resource. Further, given the lifelong incremental changes that have occurred on the Wharf, these proposed alterations are in keeping with the past accumulations of repairs, upgrades and expansions that have retained the Wharf's place as an important Santa Cruz commercial and tourist destination. The proposed Master Plan, even if realized to its fullest extent with every potential change implemented, would not result in the "substantial adverse change" in the significance of the Wharf.



State of California — The Resources Agency DEPARTMENT OF PARKS AND RECREATION PRIMARY RECORD	Primary # HRI # Trinomial NRHP Status Code	Other Listings Review Code Reviewer Date
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Page 1 of 12 *Resource Name or #: Santa Cruz Municipal Wharf

P1. Other Identifier: Santa Cruz Municipal Pier

***P2. Location:** ☐ Not for Publication ☒ Unrestricted

***a. County:** Santa Cruz

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

***b. USGS 7.5' Quad:** Santa Cruz **Date:** 1954, photorevised 1968 **T 11S ; R 2W ; SE ¼ of NE ¼ of Sec 24 ; M.D. B.M.**

c. Address: Beach Street, Municipal Wharf

City: Santa Cruz

Zip: 95060

d. UTM: Zone: 10 ; 0587189 mE/ 4090950 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate) Elevation:

The Santa Cruz Municipal Wharf is located at the southern end of Pacific Avenue, and extends out over the beach and waterway.

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Santa Cruz Municipal Wharf is situated at the southern end of Pacific Avenue, and is a prominent feature of the Santa Cruz waterfront. The approximately 3,000-foot long Wharf was constructed in 1914, with its end bent to the west for a steamship dock and freight warehouse, creating an irregular L-shape form. The structure currently stands approximately 22 feet above mean sea level. Davits for lifting fishing vessels once lined the wharf, as well as a rail line, warehouses, and fishing-related storefronts. Today, the largely industrial and commercial oriented historic-era businesses of the Wharf, have been replaced by restaurants and other tourist oriented services. The rail line was taken out ca. late 1940s to early-1950s, and replaced with a paved deck for vehicle traffic. Numerous davits for lifting fishing boats were removed after construction of the Santa Cruz Harbor in 1964. The deck of the Wharf has been widened to allow larger restaurant buildings, etc. Over 20 buildings and structures associated with these businesses are located on the deck of the Wharf, as well as other built environment features associated with utilities, lighting, etc. These auxiliary structures are not a part of this recordation.

Per the Santa Cruz Public Library website, the Municipal Wharf originally consisted of 2,043 approximately 70- foot-long Douglas fir pilings, driven 21 feet into the ocean floor. That total number of pilings has been increased to 4,528 today, with approximately 5 percent remaining from the original construction. On average, 30 pilings need to be replaced each year for safety. The Wharf has been widened from its original 100-foot width over the years to allow for larger business and more traffic. The location and length of the Wharf remain unchanged.

***P3b. Resource Attributes:** (List attributes and codes) AH13, HP11

***P4. Resources Present:** ☐ Building ☒ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)

P5b. Description of Photo: Santa Cruz Municipal Wharf, looking northeast from water.



***P6. Date Constructed/Age and Sources:**
☒ Historic: 1914

***P7. Owner and Address:**

City of Santa Cruz
809 Center Street
Santa Cruz, CA 95060

***P8. Recorded by:**

Corri Jimenez
Architectural Historian
URS Corporation
1333 Broadway Avenue, Suite 800
Oakland, CA 94612

***P9. Date Recorded:** 11-17-2011

***P10. Survey Type:** (Describe)
Reconnaissance Survey

***P11. Report Citation:** URS 2012. *Cultural Resources Study for SCWD2 Regional Seawater Desalination Project, Santa Cruz County, California*. Prepared for scwd2 Desalination Program, 212 Locust Street, Santa Cruz, CA 95060. Prepared by URS Corp., 100 W. San Fernando St., Suite 200, San Jose, CA 95113

***Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

DPR 523A (1/95)

***Required information**

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION

Primary #
HRI#

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 12

*NRHP Status Code 3CS, 5S1

*Resource Name or # Santa Cruz Municipal Wharf

B1. Historic Name: Santa Cruz Municipal Wharf

B2. Common Name: Santa Cruz Municipal Wharf

B3. Original Use: Pier for fishing industry, shipping, large boat docking

B4. Present Use: Recreational pier and tourism

*B5. Architectural Style: Utilitarian

*B6. Construction History: Initial construction in 1914. Alterations ongoing for maintenance (replacement of pilings, associated buildings, etc.). The pier originally had a railroad line that ran along the eastern side of the deck, out to the end, for shipping, etc. (which made the original 1875 Railroad Wharf obsolete for rail shipping and was torn down in 1922). The rail line was taken out ca. late 1940s to early-1950s, and replaced with a paved deck for vehicle traffic. Wharf widened to allow larger restaurant buildings, etc. Numerous davits for lifting fishing boats removed after construction of the Santa Cruz Harbor in 1964, located approximately three-quarters of a mile east of the Santa Cruz Municipal Wharf.

*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date:

Original Location:

*B8. Related Features: Numerous buildings housing restaurants and other tourist-related businesses

B9a. Architect: Unknown

b. Builder: City of Santa Cruz

*B10. Significance: Theme: Industrial Development/Transportation

Area: Local (City of Santa Cruz and vicinity)

Period of Significance: 1914-1964

Property Type: Wharf/Pier

Applicable Criteria: CRHR 1, City of Santa Cruz Registry
Criteria 1 and 2

Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Santa Cruz Municipal Wharf is the first entry in the Santa Cruz Historic Building Survey, Volume I (Page and Associates 1976:26), and was given a rating of "excellent" at the time, based on an evaluation of the structure's historical significance, architectural significance, importance to the neighborhood, alterations, and physical condition. The Wharf is also included on the City of Santa Cruz list of properties on the Historical Building Survey. The Municipal Wharf is the last in a series of six piers that were built on the Santa Cruz waterfront between 1849 and 1914. The property's period of significance spans from its construction in 1914 through 1964, when commercial and recreational fishing operations shifted permanently from the Municipal Wharf to the new Santa Cruz Harbor.

The following context for Santa Cruz' wharves and piers is excerpted from the Historic Context Statement for the City of Santa Cruz (Lehman 2000:27): (See Page 3 of 12)

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: (See Page 6 of 12)

B13. Remarks:

B14. Evaluator: Jay Rehor & Corri Jimenez, URS Corp.
1333 Broadway, Suite 800
Oakland, CA 94602

*Date of Evaluation: 12/10/2012

(This space reserved for official comments.)



Map to Scale; Santa Cruz USGS 7.5 Minute Topographic

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary #
HRI#
Trinomial

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*Resource Name or # Santa Cruz Municipal Wharf

*Recorded by: Jay Rehor & Corri Jimenez

*Date: 12/10/12

☒ Continuation

☐ Update

B10. Continued

Before railroads and motor roads linked the community to the outside world, the city's industries were dependent on shipping. Getting goods to waiting ships was the first problem and the initial solution was primitive at best. Lumber was dragged through the surf and hoisted aboard waiting schooners. Men carried lighter objects on their shoulders to small boats which in turn brought them to ships. Elihu Anthony, a merchant and foundry owner came up with a marginally better system with his partner Edwin S. Penfield when in 1847 they built a rudimentary wharf at the foot of the present Bay street. The "wharf" was really a plank chute steep enough to slide a sack of potatoes into a waiting rowboat. It was later bought by Davis and Jordan who owned a small fleet of schooners that carried shipments of lime from their kilns. They replaced the structure in 1856 with a 1000 foot wharf and developed a system wherein the lime was transported by tram cars moved by gravity down an incline, then hauled with horses back up the hill. This wharf was ultimately destroyed by heavy seas in 1907.

A second wharf was completed in 1855 by David Gharkey who extended it in 1863 to accommodate larger vessels. When a narrow gauge railroad began operating between Santa Cruz and Felton, tracks were laid and the Gharkey Wharf became the railroad wharf. It was later purchased by the South Pacific Coast railroad. Although the California Powder Works initially used the Davis Jordon wharf, they began construction on their own which was located on what is now Santa Cruz's main swimming beach. In addition they built a large warehouse on the top of Beach Hill.

For about five years, the powder and railroad wharves were connected but the cross wharf between the two was demolished in 1882. The city's fishing fleet used the railroad wharf as its base of operations but moved to the municipal wharf that was constructed by the city in 1914. The railroad wharf was finally torn down in 1922. (Refer to composite sketch of the wharf on Page 5 of 12)

The opening of the municipal wharf, built as a result of a \$172,000 bond issue, was dedicated with great fanfare on December 5, 1914. Composed of over 2000 Douglas fir pilings, the wharf was 2,745 feet long. It has been remodeled and refurbished a number of times, including a major project completed in 1984. In contrast to the city's early wharves that had warehouses and businesses associated with fishing and shipping, the current wharf is tourist oriented with restaurants, stores and souvenir shops... (Lehman 2000:27)

Within the historic context of the economic development of Santa Cruz, between 1850 and 1950, the Municipal Wharf represents an important property type related to industrial development, transportation, and recreation development (Lehman 2000:28-32). As the last of a series of six wharves that were built to serve industrial and commercial development in Santa Cruz—and which were a defining element of the Santa Cruz waterfront since 1847—the Municipal Wharf serves as a vital physical reminder of that history. As Lehman writes, "it is, perhaps, the last remnant of the time when the waterfront served the needs of a booming industrial economy" (Lehman 2000:30).

Naturally, the historical fishing industry was also heavily dependent on the wharves of Santa Cruz, including the Municipal Wharf, prior to 1950. As Lehman (2000:14) writes:

the first viable commercial fishing company was not created until a narrow gauge railroad line was built between Santa Cruz and the Southern Pacific station near Watsonville in 1875. A group of Italians, along with some Californios already living in Santa Cruz began a fresh fish business at the terminus of the railroad in Santa Cruz. In 1879, 139,000 pounds of fish were shipped from that port. Italian families, most from Genoa and the towns nearby, became associated with the fishing industry and many of their descendants are still residents of Santa Cruz. These include familiar local names like Stagnaro, Carniglia, Canepa and Faraola.

Cottardo Stagnaro arrived from Italy in 1874, and along with his sons and grandchildren, established the C. Stagnaro Fishing Corporation. In 1902, John and Sunday Faraola whose father emigrated to California in the 1860s, established a commercial fishing company on the old railroad wharf. They built a fleet of fishing vessels that was one of the largest on the Central California coast. At the height of the industry, 75 to 100 boats a day unloaded tons of salmon, sea bass rock cod and sole. Sport fishing has been a top attraction since the turn of the century and the Faraola family ran a charter service on the wharf in addition to their commercial fishing business.

World War II proved a disastrous time for the Santa Cruz fishing fleet. As a result of Executive Order #9066 issued in February 1942, Italian families, many of whom had established neighborhoods at the lower end of Bay Street near the waterfront, were made to leave their homes and move inland. Even those who had sons who were born in the United States and were serving in the armed forces, were not permitted to enter restricted areas that included the entire coastline. The boats were abandoned or confiscated for use in the war effort. After the war, many of the fishing families became involved in other business pursuits. Although Santa Cruz no longer has an active commercial fishing fleet based in the city, the sport fishing business is still active and the names of pioneering Italian families can be seen on the concessions and restaurants lining the present day municipal wharf. (See Page 4 of 12)

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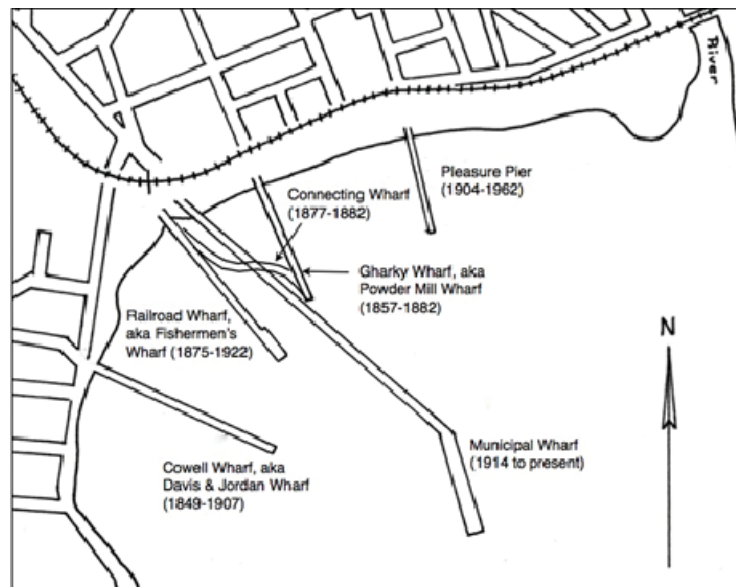
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*Date: 12/10/12

☒ Continuation

☐ Update

B10. Continued



Composite sketch map of all of Santa Cruz's historic wharves (Frank Perry Collection; <http://limekilnlegacies.com/pdfs/Notes-on-Santa-Cruz-Wharves.pdf>)

CRHR and City of Santa Cruz Registry

The Santa Cruz Municipal Wharf has historical significance on a local level under Criterion 1 of the CRHR as well as City of Santa Cruz Registry Criteria 1 and 2 due to its association with the economic development of Santa Cruz and the long history and role of wharves along the Santa Cruz waterfront. The wharf has been continuously used since its original construction in 1914 and, although it has undergone numerous functional and structural changes since that time, remains a vital part of the fabric of the Santa Cruz waterfront. One of the largest of these changes occurred in 1964, when construction of the Santa Cruz Harbor was completed, and commercial and recreational fishing operations shifted permanently from the Municipal Wharf to the new harbor. However, even prior to this time, the Wharf had begun to make the transition to the service and tourist industry, as attested in historic photographs from the 1950s, which show the original railroad absent, a paved deck for vehicles, and restaurants beginning to dominate the Wharf (See Photo on Page 8 of 12).

While many of the original materials, both above and below deck, have been replaced over time in order to maintain the Municipal Wharf as a functioning structure (and, as such, lacks integrity of materials and workmanship), the Wharf does maintain its integrity of location, design, setting, feeling, and association. The Wharf relays a sense of location, as it is still located in the place where it was originally built in 1914 at the base of Pacific Avenue, which historically provided a direct connection between the Wharf and downtown Santa Cruz. In terms of design, the Municipal Wharf still reflects the intent of the Wharf to provide Santa Cruz with a direct connection to ocean industries and shipping, despite the fact that the function of the Wharf significantly changed after ca. 1950. This historical connection is further enhanced by the physical environmental setting on the Santa Cruz waterfront, and the Wharf's prominence in views from West Cliff and Beach Hill. (See Page 5 of 12)

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B10. Continued

Along with other historic built environment resources such as the Boardwalk, the Municipal Wharf helps convey the sense of place and orientation of Santa Cruz along the waterfront which was integral in the development of the city. The Wharf retains its integrity of feeling since it still retains its significant physical characteristics that convey its historic qualities, and evoke a sense of its historical past (which spans from 1914 through 1964). The Wharf retains its integrity of association as the property is directly linked to past significant events, such as its use in shipping and fishing industry. While many of the original materials and fabric have been replaced as part of continued maintenance to sustain the property's use, the changes have not affected the character or visual narrative of the property and have enabled retention of integrity of setting, feeling, and association. The majority of the businesses currently on the Wharf have been present for decades, particularly the Italian restaurants. These restaurants do not reflect the original fishing-related warehouses and buildings that historically occupied the Wharf, but are indicative of the early Italian-American adoption of the Wharf as a place of business, as well as the shift from primary fishing to tourist enterprises by those early families.

In addition to the above criteria, when analyzing the historical significance of a property with a period of significance that extends into the recent past, or less than 50 years from the present, Criterion Consideration G of the National Register must also be applied. Typically the National Register Criteria for Evaluation exclude properties that achieved significance within the past fifty years unless they are of exceptional importance. Fifty years is a general estimate of the time needed to develop historical perspective and to evaluate significance. This consideration guards against the listing of properties of passing contemporary interest and ensures that the National Register is a list of truly historic places. The Office of Historic Preservation expects this guidance to be applied to CRHR evaluations.

According to the National Park Service, the phrase "exceptional importance" may be applied to the extraordinary importance of an event or to an entire category of resources so fragile that survivors of any age are unusual. Properties less than fifty years old that may qualify as exceptional include those where the entire category of the resource is fragile. The phrase "exceptional importance" does not require that the property be of national significance. It is a measure of a property's importance within the appropriate historic context, whether the scale of that context is local, state, or national.

The Santa Cruz Municipal Wharf is the last wharf of the original six that were constructed, and is the only surviving property associated with the shipping and fishing industries of the City from the early 20th century. Although, as indicated by the City of Santa Cruz website, fewer than 5 percent of the original pilings still exist at the Santa Cruz Municipal Wharf and there has been additions and alterations to the structure within the past fifty years, which has led to major losses in historic fabric and materials, it still conveys its historical significance, especially as the last surviving example of a fragile property type.

In summary, the Santa Cruz Municipal Wharf appears to be eligible to the CRHR under Criteria 1 and the City of Santa Cruz Registry under Criteria 1 and 2, and retains its historic integrity. In addition, as a property that continued to achieve significance into a period less than fifty years ago, the Santa Cruz Municipal Wharf is an exceptional example of a fragile category of properties under Criterion Consideration G. Therefore, the property is considered a historical resource for purposes of CEQA.

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B12. Continued

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Early photo of Municipal Wharf, with railroad tracks at right. Original 1875 Railroad Wharf barely visible in background at left; small Pleasure Pier (part of Boardwalk) visible in background at right (Frank Perry Collection; <http://limekilnlegacies.com/pdfs/Notes-on-Santa-Cruz-Wharves.pdf>)



Ca. 1914 photo of new Municipal Wharf (left) and Railroad Wharf (right) (Santa Cruz Public Library Collection)

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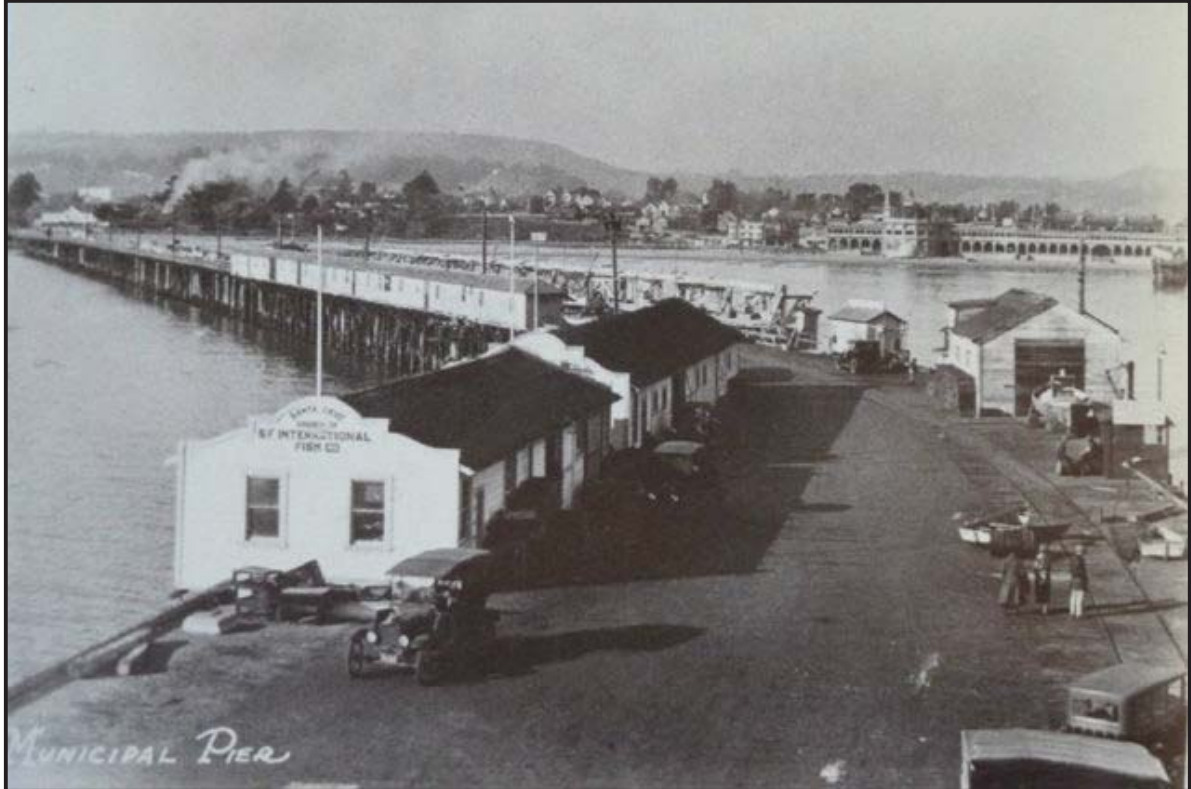
*Resource Name or # Santa Cruz Municipal Wharf

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Ca. 1920s photo of Municipal Wharf, with railroad tracks at right. Note businesses lining west side of pier and davits on east side, similar to the present-day layout (Santa Cruz Museum of Natural History)



Ca. 1950s photo of Municipal Wharf. Railroad tracks removed; fishing fleet present on east side of pier (Santa Cruz Museum of Natural History)

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View to the East: Current view of Municipal Wharf from West Cliff Drive.



View to the West: Aerial view of Municipal Wharf in context of Santa Cruz waterfront (Beach Hill and Boardwalk in foreground; West Cliff in background).

(Photo courtesy of Robert Campbell, [http://commons.wikimedia.org/wiki/File%3AAerial view - Santa Cruz CA.jpg](http://commons.wikimedia.org/wiki/File%3AAerial_view_-_Santa_Cruz_CA.jpg))

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View to the North: Commercial Buildings, built along Wharf roadway.



View to the West: Commercial Buildings, built along Wharf roadway.

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View to the West: Commercial Buildings, built along Wharf roadway.



View to the West: Commercial Buildings, built along Wharf roadway.

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LOCATION MAP

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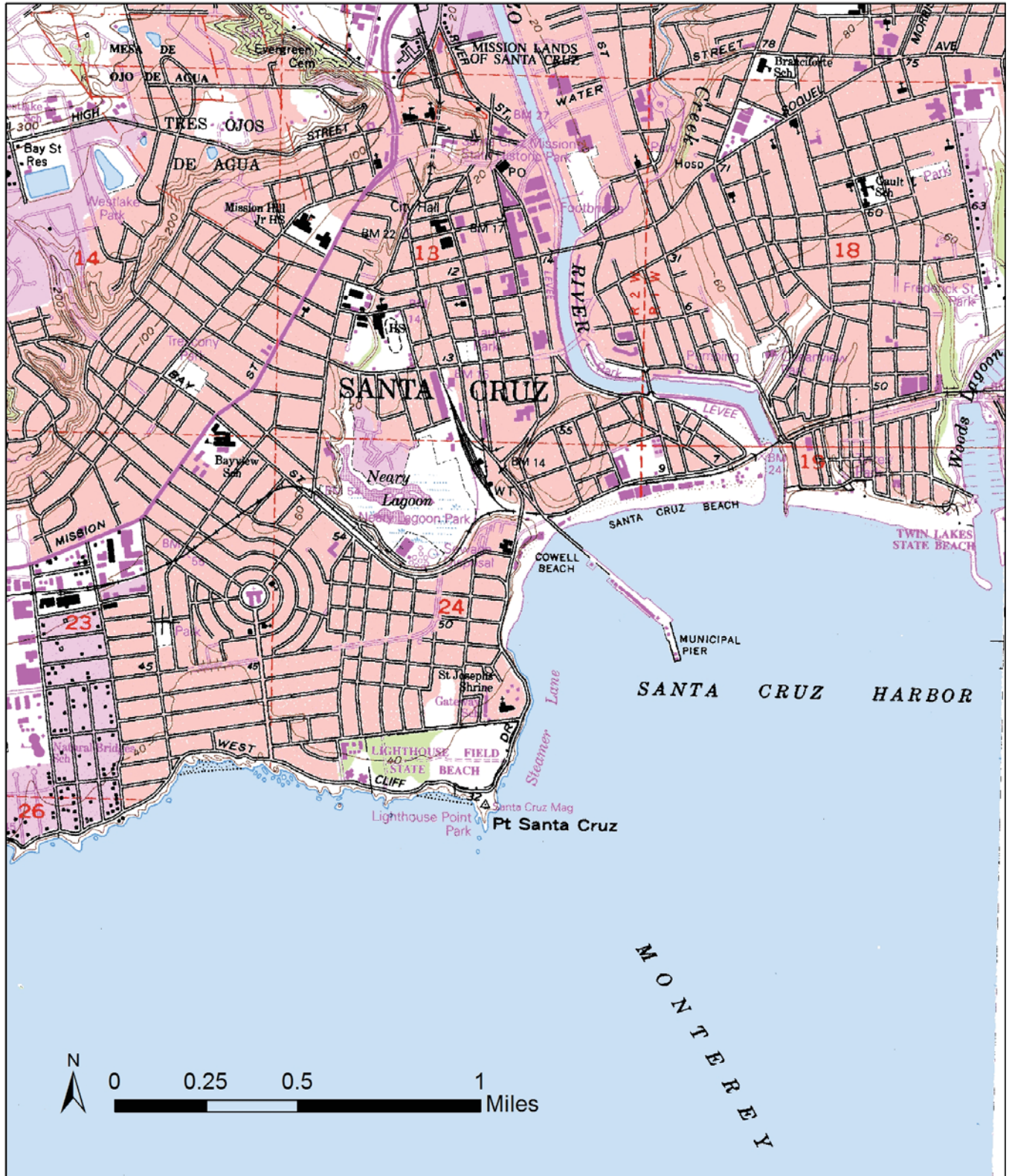
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***Resource Name or #:** Santa Cruz Municipal Wharf

***Map Name:** Santa Cruz USGS 7.5 Minute Topographic

***Scale:** 1:24,000 ***Date of Map:** 1954 (P.R. 1968)



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***Resource Name or #** Santa Cruz Municipal Wharf

***Recorded by:** Fallin Steffen, Dudek ***Date:** November 15, 2019

☐ Continuation ☒ Update

P1. Other Identifiers:

***P2. Location:** ☐ Not for Publication ☒ Unrestricted ***a. County:** Santa Cruz

***b. USGS 7.5' Quad:** Santa Cruz, CA **Date:** Rev. 2018 **T** 11S **R** 2 W, 1 W **SEC** 13, 19, 24; Mount Diablo **B.M.**

c. Address: **City:** Santa Cruz **Zip:** 95060

d. UTM: Zone: 10 S, 587081.35mE/ 4091044.06mN

e. Other Locational Data: The Santa Cruz Municipal Wharf is sited at Municipal Wharf Street in the City of Santa Cruz (City). The irregular, L-shaped structure extends from the southern end of Front Street into the Monterey Bay. The site is bounded by Cowell Beach and the Monterey Bay to the west, Beach Street to the north, Santa Cruz Beach and the Monterey Bay to the east, and the Monterey Bay to the south. See **Aerial View** on Page 2.

***P3a. Description:** The purpose of this update is to reflect the evaluation of the property for the National Register of Historic Places (NRHP) which was prepared by Dudek as part of the Santa Cruz Wharf Maintenance project as an adjunct to existing documentation for the property prepared in 2011 by the URS Corporation. The 2011 documentation effort only evaluated the property for California Register of Historic Places and Local significance criteria (see 2011 DPR523 attached).

The Santa Cruz Municipal Wharf: The subject property is defined as the extent of the approximately 7.5-acres, irregular L-shaped, wooden structure which extends 3,000-ft into the Monterey Bay at the southern end of Front Street (Figure 1). The structure was designed by structural engineer, Henry John Brunnier and completed in 1914. It is supported by a complex arrangement of approximately 4,500 treated, wooden piles that extend below the surface of the bay and are secured into the ocean floor (Figure 2). The visible asphalt-paved deck of the wharf sits atop wide wooden planks supported by horizontal stringers (Figure 3).

The entrance onto the wharf is guarded by two parking kiosks of recent construction located just off the traffic circle at the intersection of Front, Beach, and Municipal Wharf Streets (Figure 4). The structure is accessed via two paved pedestrian walkways lining the west and east side of the structure (Figure 5), as well as by vehicle along the two, paved, central lanes which run the length of the structure (Figure 6). The wharf varies in width from section to section and new sections have been added through the years. (*Continued on Page 3*)

***P3b. Resource Attributes:** (List attributes and codes) HP11- Engineering Structure, AH13 - Wharf

P5b. Description of Photo: View of the Wharf looking south from Santa Cruz Beach. November 15, 2019 (DSCN4145)



***P6. Date Constructed/Age and Sources:**

☒ Historic ☐ Prehistoric ☐ Both
1914
(The Californian 1914:1)

***P8. Recorded by:**

(Name, affiliation, address)
Fallin Steffen, MPS
Dudek
725 Front Street, Suite 400
Santa Cruz, CA.95060

***P11. Report Citation:** Dudek. 2019.

*Cultural Resources Inventory And
Evaluation Report For The
Santa Cruz Municipal Wharf Maintenance
Project.*

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***B6. Construction History:** (Construction date, alternation, and date of alternations): The structure was designed by structural engineer, Henry John Brunnier and completed in 1914. The following list of known alterations was compiled through archival research, a review of previous subject property documentation, and during the course of the pedestrian survey. Unless indicated, the dates of these alterations is not known:

Deck Surface: Although the length and overall shape of the wharf has not changed since 1914, sections of the wharf have been gradually widened over time to accommodate more parking, new uses and larger commercial establishments (Figure 11).
(Continued on Page 8)

***B10. Significance:** This NRHP evaluation supplements the 2011 evaluation of the Wharf by URS that found the wharf structure significant for listing in CRHR 1, and under Santa Cruz Criteria. The Santa Cruz Wharf also appears eligible for listing in the NRHP under Criterion A for its continued associations with the industrial development, transportation, commerce, and recreation development of the City. Additionally, it appears eligible under NRHP Criterion C as a distinctive engineering structure known to be the longest wooden pier structure located along the United States Coast of the Pacific Ocean. As a NRHP eligible property the Wharf will be automatically listed in the CRHR, as such it should be considered CRHR eligible under Criteria 1 and 3. The Santa Cruz Wharf was evaluated in accordance with Section 15064.5(a) (2)-(3) of the California Environmental Quality Act (CEQA) Guidelines using the criteria outlined in Section 5024.1 of the California Resources Code, and it appears to be a historical resource for the purposes of CEQA. Full DPRs prepared by Dudek and the 2011 evaluation by URS are located in Appendix C. (Continued on Page 8).

Aerial View (Sketch Map). This image shows the historic property boundary in red. Photograph courtesy of Google Earth image date 2019, downloaded on December 18, 2019.



***B12. References:**

- H.J. Brunnier Associates. 1997. "History of H.J. Brunnier Associates." Online Article. Accessed November 22, 2019. www.hjbrunnier.com/history.html
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***P3a. Description:** Multiple asphalt repair campaigns are visible across the various sections of the structure as are the stress lines in the surface asphalt from the movement of the wharf (Figure 7). Diagonal and perpendicular parking spaces (with circulation lanes in the wider areas of the wharf) line the length of the structure (Figure 8), except for the narrow section located closest to the shore and the section located at the very end. A, pressure-treated, squared wooden railing with a unique gable top secures the entire perimeter of the wharf, and also surrounds the five, square fishing access portals cut out from the central deck of the wharf's end section (Figure 9).

The Wharf has been modernized over time to serve the uses of the Santa Cruz community. The deck of the wharf contains approximately twenty buildings and structures mainly associated with the commercial uses of the wharf including tourism, hospitality and food service (Figure 10). Historic aerials indicate that these building generally date to 1968 to 1982, with a few buildings added after the early 1980s. Other extant built modern features on the deck of the wharf include utility housing, lighting infrastructure and ancillary buildings related to wharf maintenance.



Figure 1. Santa Cruz Municipal Wharf, view looking west (DSCN4143).



Figure 2. Pile construction on the northeastern side of the wharf (View looking west, DSCN4106)

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Figure 3. Horizontal stringers supporting layered deck construction (Detail, DSCN 4109)



Figure 4. Parking kiosks located at wharf entrance off the traffic circle located at the intersection of Beach-Front and Municipal Wharf Streets (View looking southeast, DSCN 4146)

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Figure 5. View of paved pedestrian walkway along edges of wharf (View looking northwest, DSCN 4126)



Figure 6. Paved vehicular traffic lanes on wharf deck (View looking southeast, DSCN 4129)

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Figure 7. Stress lines in the surface asphalt from the movement of the wharf (View looking east, DSCN 4101)



Figure 8. Angled (left) and perpendicular (right) parking areas with corresponding circulation lanes (View looking northwest, DSCN 4093)

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Figure 9. Example of the wooden railing protecting the perimeter of the wharf. Note the fishing access portals at center-left of the photo which are enclosed with the same style wooden railing (View looking north, DSCN 4047)



Figure 10. An example of the commercial buildings lining the wharf (View looking west, DSCN 4107)

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***B6. Construction History:**

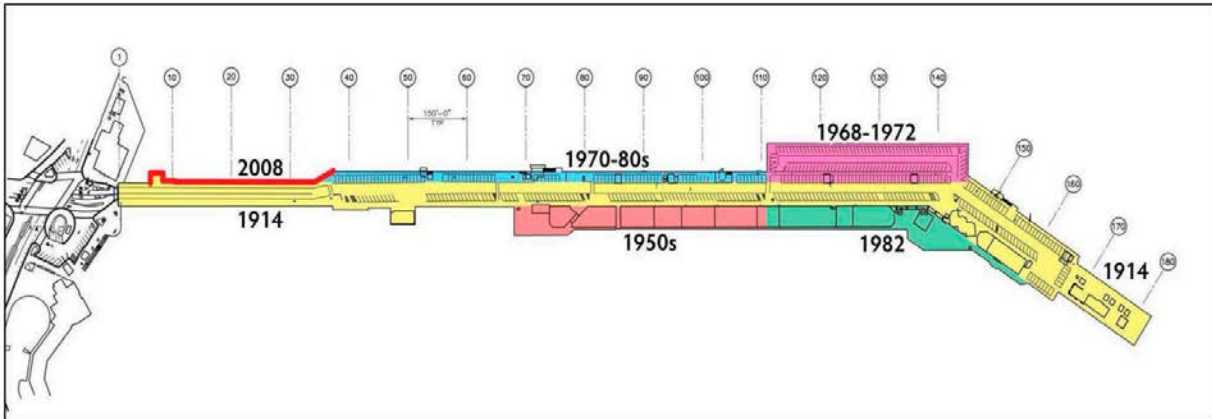


Figure 11. Graphic showing the expansion of the Wharf overtime.

Materials: The structural elements supporting the wharf have been replaced on a routine basis due to normal wear and tear associated with the use of the wharf. This includes the replacement of piles, stringers, decking and asphalt.

Rail Spur: The original rail spur which ran the length of the wharf was removed at some point during the mid-Twentieth century.

Railing: The railing is not original to the wharf and has additionally been subject to routine material repairs and replacements since its installation.

Pavement: The asphalt covering the deck of the wharf is not original and has been subsequently replaced and repaired in multiple areas

***B10. Significance:**

NRHP Statement of Significance

Criterion A: That are associated with events that have made a significant contribution to the broad patterns of our history.

The Historic Context for the City of Santa Cruz (Lehmann 2000:28—32) indicates that the Municipal Wharf, which is actually a pier structure, is the last and only remaining of six similar structures built between 1847 and 1914 in the City of Santa Cruz. These piers are fundamentally linked to the early industrial, transportation, and recreational development and growth of the City. Prior to the introduction of the railroad and the advent of private vehicular travel, the early economic industries in Santa Cruz producing lumber and hydraulic lime relied on shipping as a means of distribution to consumer markets elsewhere. As a result, these structures became a prominent and omnipresent defining feature of the coastline in Santa Cruz.

The Santa Cruz Municipal Wharf was developed in 1914 with municipal funds as the last of the six piers in Santa Cruz. Lehmann notes that it was intended to represent an evolutionary fusion between the shipping and tourism industries within Santa Cruz because "In contrast to the city's early wharves that had warehouses and businesses associated with fishing and shipping, the current wharf [was intended to be] tourist oriented with restaurants, stores and souvenir shops (Lehman 2000: 27)." However, the alignment of the new wharf made it an appealing docking location for the commercial fishing industry in Santa Cruz, who soon took advantage of the functional location of the new wharf for off-loading daily catches. The fishing industry remained centered around the Wharf until the development of the Santa Cruz Harbor in 1964, which permanently shifted the industry away from the Wharf.

The Santa Cruz Municipal Wharf therefore survives as the last wharf constructed along the Santa Cruz coastline between 1847 and 1914 to serve the economic and commercial ventures of the City. It has remained in continuous use since the time

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of its construction, and although the commercial fishing industry is no longer located on the Wharf, the name of at least one pioneering nineteenth century Italian fishing family, Stagnaro, is represented on one of the current restaurants lining the wharf. Therefore, as Lehman suggests, "it is, perhaps, the last remnant of the time when the waterfront served the needs of a booming industrial economy [in Santa Cruz] (Lehman 2000:30)."

Therefore, the Santa Cruz Municipal Wharf appears eligible under NRHP Criterion A as the last built representative of the important function that shipping played in the early economic growth and the sustained development of the City of Santa Cruz.

Period of Significance: 1914-1964

Criterion B: That are associated with the lives of persons significant in our past.

To be found eligible under B/2 the property has to be directly tied to an important person and the place where that individual conducted or produced the work for which he or she is known. Archival research failed to indicate any such direct association with individuals are known to be historic figures at the national, state, or local level and the Santa Cruz Municipal Wharf. As such, the structure is not known to have any historical associations with people important to the nation's or state's past. Due to a lack of identified significant associations with important persons in history, the Santa Cruz Wharf does not appear eligible under NRHP Criterion B.

Criterion C: That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction.

The L-shaped, Santa Cruz Municipal Wharf was completed in December 1914. A structural engineer from San Francisco named Henry John Brunnier designed the wharf and oversaw its construction. Brunnier was responsible for the design of other prominent wharf projects in Northern California, including the San Francisco Waterfront and Embarcadero. Outside of his work in wharf design, Brunnier provided the structural design of many iconic buildings and structures in San Francisco and sat on the board of structural engineers who oversaw the design and construction of the San Francisco to Oakland Bay Bridge. It appears that Brunnier rises to the level of Master engineer. However, in light of Brunnier's work on other, more prominent wharf projects such as the San Francisco Waterfront and Embarcadero, the Santa Cruz Municipal Wharf cannot be called the most prestigious example of a Brunnier design, nor is it the first or last example of Brunnier's work.

In order to accommodate additional commercial uses and parking, multiple additions have expanded the original 4.2-acres of surface area of the wharf to approximately 7.5-acres since 1914. Despite this, the wharf still displays several of the distinctive features of the original design including: irregular L-Shape; wooden construction; and location and orientation to the shoreline. It also maintains its original length, measuring 2,745 feet. The Wharf presently constitutes the longest example of a wooden pier structure in California. It is also the longest pier of any kind located on the United States Coast Pacific Ocean coastline (See-California 2019).

Although the Santa Cruz Wharf is not representative of Brunnier's more prominent wharf projects, it is a distinctive engineering structure as the longest wooden pier structure located on the Pacific Ocean. As such the Santa Cruz Wharf appears eligible for listing in the NRHP under Criterion C.

Criterion D: That have yielded, or may be likely to yield, information important in prehistory or history.

There is no evidence to suggest that this property has the potential to yield information important to state or local history. Therefore, the property is recommended not eligible under NRHP Criterion D.

Integrity Discussion

In addition to meeting one or more of the above criteria, an eligible resource must retain integrity, which is expressed in seven aspects: location, design, setting, workmanship, materials, feeling, and association. All properties change over the course of time. Consequently, it is not necessary for a property to retain all its historic physical features or characteristics. The property must retain, however, the essential physical features that enable it to convey its historic identity. The essential physical features are those features that define both why a property is significant and when it was significant.

State of California & Natural Resources Agency
DEPARTMENT OF PARKS AND RECREATION

UPDATE SHEET

Primary#
HRI # 014092
Trinomial
NRHP Status Code: 3S

Property Name: Santa Cruz Municipal Wharf
Page 10 of 10

In the case of the Santa Cruz Wharf, the structure's period of significance is 1914, the year it was built. The structure is still located in its historic setting, retains its historic alignment, its original length of 2,745 feet; wooden materials and its Irregular L-Shape have been maintained. Additionally the structure continues to function as wharf. As such, despite ongoing maintenance improvements, and the addition of mid to late 20th Century buildings and infrastructure to the deck, , the Santa Cruz Wharf appears to retain a high degree of integrity of setting, and location, and sufficient degree of design, materials, workmanship, feeling, and association.

Character Character-Defining Features

The character character-defining features associated with the Santa Cruz Wharf are its location, setting, alignment, wooden materials (piers), its original length of 2,745 feet long and its continued function as a wharf structure. The contemporary infrastructural elements on the wharf deck including roads, lighting, utilities, and buildings do not date to the 1914 period of significance identified for the Santa Cruz Wharf. As such they and are considered non-contributing elements of the structure.

Historic Property Boundary

The historic property boundary for the Wharf is limited to the 1914 portion of the structure illustrated in Figure 11 in yellow.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary #

HRI #

Trinomial

NRHP Status Code

Other Listings
Review Code

Reviewer

Date

Page 1 of 12

*Resource Name or #: Santa Cruz Municipal Wharf

P1. Other Identifier: Santa Cruz Municipal Pier

*P2. Location: ☐ Not for Publication ☒ Unrestricted

*a. County: Santa Cruz

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad: Santa Cruz Date: 1954, photorevised 1968 T 11S ; R 2W ; SE ¼ of NE ¼ of Sec 24 ; M.D. B.M.

c. Address: Beach Street, Municipal Wharf

City: Santa Cruz

Zip: 95060

d. UTM: Zone: 10 ; 0587189 mE/ 4090950 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate) Elevation:

The Santa Cruz Municipal Wharf is located at the southern end of Pacific Avenue, and extends out over the beach and waterway.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Santa Cruz Municipal Wharf is situated at the southern end of Pacific Avenue, and is a prominent feature of the Santa Cruz waterfront. The approximately 3,000-foot long Wharf was constructed in 1914, with its end bent to the west for a steamship dock and freight warehouse, creating an irregular L-shape form. The structure currently stands approximately 22 feet above mean sea level. Davits for lifting fishing vessels once lined the wharf, as well as a rail line, warehouses, and fishing-related storefronts. Today, the largely industrial and commercial oriented historic-era businesses of the Wharf, have been replaced by restaurants and other tourist oriented services. The rail line was taken out ca. late 1940s to early-1950s, and replaced with a paved deck for vehicle traffic. Numerous davits for lifting fishing boats were removed after construction of the Santa Cruz Harbor in 1964. The deck of the Wharf has been widened to allow larger restaurant buildings, etc. Over 20 buildings and structures associated with these businesses are located on the deck of the Wharf, as well as other built environment features associated with utilities, lighting, etc. These auxiliary structures are not a part of this recordation.

Per the Santa Cruz Public Library website, the Municipal Wharf originally consisted of 2,043 approximately 70- foot-long Douglas fir pilings, driven 21 feet into the ocean floor. That total number of pilings has been increased to 4,528 today, with approximately 5 percent remaining from the original construction. On average, 30 pilings need to be replaced each year for safety. The Wharf has been widened from its original 100-foot width over the years to allow for larger business and more traffic. The location and length of the Wharf remain unchanged.

*P3b. Resource Attributes: (List attributes and codes) AH13, HP11

*P4. Resources Present: ☐ Building ☒ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)

P5b. Description of Photo: Santa Cruz Municipal Wharf, looking northeast from water.

*P6. Date Constructed/Age and Sources:
☒ Historic: 1914

*P7. Owner and Address:

City of Santa Cruz
809 Center Street
Santa Cruz, CA 95060

*P8. Recorded by:

Corri Jimenez
Architectural Historian
URS Corporation
1333 Broadway Avenue, Suite 800
Oakland, CA 94612

*P9. Date Recorded: 11-17-2011

*P10. Survey Type: (Describe)
Reconnaissance Survey

*P11. Report Citation: URS 2012. *Cultural Resources Study for SCWD2 Regional Seawater Desalination Project, Santa Cruz County, California*. Prepared for scwd2 Desalination Program, 212 Locust Street, Santa Cruz, CA 95060. Prepared by URS Corp., 100 W. San Fernando St., Suite 200, San Jose, CA 95113

*Attachments: ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

DPR 523A (1/95)

*Required information

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION

Primary #
HRI#

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 12

*NRHP Status Code 3CS, 5S1

*Resource Name or # Santa Cruz Municipal Wharf

B1. Historic Name: Santa Cruz Municipal Wharf

B2. Common Name: Santa Cruz Municipal Wharf

B3. Original Use: Pier for fishing industry, shipping, large boat docking

B4. Present Use: Recreational pier and tourism

*B5. Architectural Style: Utilitarian

*B6. Construction History: Initial construction in 1914. Alterations ongoing for maintenance (replacement of pilings, associated buildings, etc.). The pier originally had a railroad line that ran along the eastern side of the deck, out to the end, for shipping, etc. (which made the original 1875 Railroad Wharf obsolete for rail shipping and was torn down in 1922). The rail line was taken out ca. late 1940s to early-1950s, and replaced with a paved deck for vehicle traffic. Wharf widened to allow larger restaurant buildings, etc. Numerous davits for lifting fishing boats removed after construction of the Santa Cruz Harbor in 1964, located approximately three-quarters of a mile east of the Santa Cruz Municipal Wharf.

*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date:

Original Location:

*B8. Related Features: Numerous buildings housing restaurants and other tourist-related businesses

B9a. Architect: Unknown

b. Builder: City of Santa Cruz

*B10. Significance: Theme: Industrial Development/Transportation

Area: Local (City of Santa Cruz and vicinity)

Period of Significance: 1914-1964

Property Type: Wharf/Pier

Applicable Criteria: CRHR 1, City of Santa Cruz Registry
Criteria 1 and 2

Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Santa Cruz Municipal Wharf is the first entry in the Santa Cruz Historic Building Survey, Volume I (Page and Associates 1976:26), and was given a rating of "excellent" at the time, based on an evaluation of the structure's historical significance, architectural significance, importance to the neighborhood, alterations, and physical condition. The Wharf is also included on the City of Santa Cruz list of properties on the Historical Building Survey. The Municipal Wharf is the last in a series of six piers that were built on the Santa Cruz waterfront between 1849 and 1914. The property's period of significance spans from its construction in 1914 through 1964, when commercial and recreational fishing operations shifted permanently from the Municipal Wharf to the new Santa Cruz Harbor.

The following context for Santa Cruz' wharves and piers is excerpted from the Historic Context Statement for the City of Santa Cruz (Lehman 2000:27): (See Page 3 of 12)

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References: (See Page 6 of 12)

B13. Remarks:

B14. Evaluator: Jay Rehor & Corri Jimenez, URS Corp.
1333 Broadway, Suite 800
Oakland, CA 94602

*Date of Evaluation: 12/10/2012

(This space reserved for official comments.)



Map to Scale; Santa Cruz USGS 7.5 Minute Topographic

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

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*Resource Name or # Santa Cruz Municipal Wharf

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*Date: 12/10/12

☒ Continuation

☐ Update

B10. Continued

Before railroads and motor roads linked the community to the outside world, the city's industries were dependent on shipping. Getting goods to waiting ships was the first problem and the initial solution was primitive at best. Lumber was dragged through the surf and hoisted aboard waiting schooners. Men carried lighter objects on their shoulders to small boats which in turn brought them to ships. Elihu Anthony, a merchant and foundry owner came up with a marginally better system with his partner Edwin S. Penfield when in 1847 they built a rudimentary wharf at the foot of the present Bay street. The "wharf" was really a plank chute steep enough to slide a sack of potatoes into a waiting rowboat. It was later bought by Davis and Jordan who owned a small fleet of schooners that carried shipments of lime from their kilns. They replaced the structure in 1856 with a 1000 foot wharf and developed a system wherein the lime was transported by tram cars moved by gravity down an incline, then hauled with horses back up the hill. This wharf was ultimately destroyed by heavy seas in 1907.

A second wharf was completed in 1855 by David Gharkey who extended it in 1863 to accommodate larger vessels. When a narrow gauge railroad began operating between Santa Cruz and Felton, tracks were laid and the Gharkey Wharf became the railroad wharf. It was later purchased by the South Pacific Coast railroad. Although the California Powder Works initially used the Davis Jordon wharf, they began construction on their own which was located on what is now Santa Cruz's main swimming beach. In addition they built a large warehouse on the top of Beach Hill.

For about five years, the powder and railroad wharves were connected but the cross wharf between the two was demolished in 1882. The city's fishing fleet used the railroad wharf as its base of operations but moved to the municipal wharf that was constructed by the city in 1914. The railroad wharf was finally torn down in 1922. (Refer to composite sketch of the wharf on Page 5 of 12)

The opening of the municipal wharf, built as a result of a \$172,000 bond issue, was dedicated with great fanfare on December 5, 1914. Composed of over 2000 Douglas fir pilings, the wharf was 2,745 feet long. It has been remodeled and refurbished a number of times, including a major project completed in 1984. In contrast to the city's early wharves that had warehouses and businesses associated with fishing and shipping, the current wharf is tourist oriented with restaurants, stores and souvenir shops... (Lehman 2000:27)

Within the historic context of the economic development of Santa Cruz, between 1850 and 1950, the Municipal Wharf represents an important property type related to industrial development, transportation, and recreation development (Lehman 2000:28-32). As the last of a series of six wharves that were built to serve industrial and commercial development in Santa Cruz—and which were a defining element of the Santa Cruz waterfront since 1847—the Municipal Wharf serves as a vital physical reminder of that history. As Lehman writes, "it is, perhaps, the last remnant of the time when the waterfront served the needs of a booming industrial economy" (Lehman 2000:30).

Naturally, the historical fishing industry was also heavily dependent on the wharves of Santa Cruz, including the Municipal Wharf, prior to 1950. As Lehman (2000:14) writes:

the first viable commercial fishing company was not created until a narrow gauge railroad line was built between Santa Cruz and the Southern Pacific station near Watsonville in 1875. A group of Italians, along with some Californios already living in Santa Cruz began a fresh fish business at the terminus of the railroad in Santa Cruz. In 1879, 139,000 pounds of fish were shipped from that port. Italian families, most from Genoa and the towns nearby, became associated with the fishing industry and many of their descendants are still residents of Santa Cruz. These include familiar local names like Stagnaro, Carniglia, Canepa and Faraola.

Cottardo Stagnaro arrived from Italy in 1874, and along with his sons and grandchildren, established the C. Stagnaro Fishing Corporation. In 1902, John and Sunday Faraola whose father emigrated to California in the 1860s, established a commercial fishing company on the old railroad wharf. They built a fleet of fishing vessels that was one of the largest on the Central California coast. At the height of the industry, 75 to 100 boats a day unloaded tons of salmon, sea bass rock cod and sole. Sport fishing has been a top attraction since the turn of the century and the Faraola family ran a charter service on the wharf in addition to their commercial fishing business.

World War II proved a disastrous time for the Santa Cruz fishing fleet. As a result of Executive Order #9066 issued in February 1942, Italian families, many of whom had established neighborhoods at the lower end of Bay Street near the waterfront, were made to leave their homes and move inland. Even those who had sons who were born in the United States and were serving in the armed forces, were not permitted to enter restricted areas that included the entire coastline. The boats were abandoned or confiscated for use in the war effort. After the war, many of the fishing families became involved in other business pursuits. Although Santa Cruz no longer has an active commercial fishing fleet based in the city, the sport fishing business is still active and the names of pioneering Italian families can be seen on the concessions and restaurants lining the present day municipal wharf. (See Page 4 of 12)

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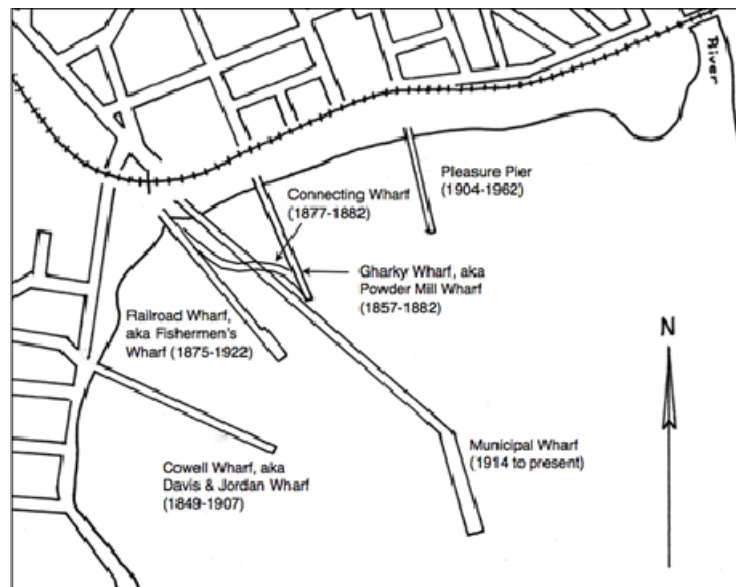
*Recorded by: Jay Rehor & Corri Jimenez

*Date: 12/10/12

☒ Continuation

☐ Update

B10. Continued



Composite sketch map of all of Santa Cruz's historic wharves (Frank Perry Collection; <http://limekilnlegacies.com/pdfs/Notes-on-Santa-Cruz-Wharves.pdf>)

CRHR and City of Santa Cruz Registry

The Santa Cruz Municipal Wharf has historical significance on a local level under Criterion 1 of the CRHR as well as City of Santa Cruz Registry Criteria 1 and 2 due to its association with the economic development of Santa Cruz and the long history and role of wharves along the Santa Cruz waterfront. The wharf has been continuously used since its original construction in 1914 and, although it has undergone numerous functional and structural changes since that time, remains a vital part of the fabric of the Santa Cruz waterfront. One of the largest of these changes occurred in 1964, when construction of the Santa Cruz Harbor was completed, and commercial and recreational fishing operations shifted permanently from the Municipal Wharf to the new harbor. However, even prior to this time, the Wharf had begun to make the transition to the service and tourist industry, as attested in historic photographs from the 1950s, which show the original railroad absent, a paved deck for vehicles, and restaurants beginning to dominate the Wharf (See Photo on Page 8 of 12).

While many of the original materials, both above and below deck, have been replaced over time in order to maintain the Municipal Wharf as a functioning structure (and, as such, lacks integrity of materials and workmanship), the Wharf does maintain its integrity of location, design, setting, feeling, and association. The Wharf relays a sense of location, as it is still located in the place where it was originally built in 1914 at the base of Pacific Avenue, which historically provided a direct connection between the Wharf and downtown Santa Cruz. In terms of design, the Municipal Wharf still reflects the intent of the Wharf to provide Santa Cruz with a direct connection to ocean industries and shipping, despite the fact that the function of the Wharf significantly changed after ca. 1950. This historical connection is further enhanced by the physical environmental setting on the Santa Cruz waterfront, and the Wharf's prominence in views from West Cliff and Beach Hill. (See Page 5 of 12)

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*Resource Name or # Santa Cruz -Municipal Wharf

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☒ Continuation

☐ Update

B10. Continued

Along with other historic built environment resources such as the Boardwalk, the Municipal Wharf helps convey the sense of place and orientation of Santa Cruz along the waterfront which was integral in the development of the city. The Wharf retains its integrity of feeling since it still retains its significant physical characteristics that convey its historic qualities, and evoke a sense of its historical past (which spans from 1914 through 1964). The Wharf retains its integrity of association as the property is directly linked to past significant events, such as its use in shipping and fishing industry. While many of the original materials and fabric have been replaced as part of continued maintenance to sustain the property's use, the changes have not affected the character or visual narrative of the property and have enabled retention of integrity of setting, feeling, and association. The majority of the businesses currently on the Wharf have been present for decades, particularly the Italian restaurants. These restaurants do not reflect the original fishing-related warehouses and buildings that historically occupied the Wharf, but are indicative of the early Italian-American adoption of the Wharf as a place of business, as well as the shift from primary fishing to tourist enterprises by those early families.

In addition to the above criteria, when analyzing the historical significance of a property with a period of significance that extends into the recent past, or less than 50 years from the present, Criterion Consideration G of the National Register must also be applied. Typically the National Register Criteria for Evaluation exclude properties that achieved significance within the past fifty years unless they are of exceptional importance. Fifty years is a general estimate of the time needed to develop historical perspective and to evaluate significance. This consideration guards against the listing of properties of passing contemporary interest and ensures that the National Register is a list of truly historic places. The Office of Historic Preservation expects this guidance to be applied to CRHR evaluations.

According to the National Park Service, the phrase "exceptional importance" may be applied to the extraordinary importance of an event or to an entire category of resources so fragile that survivors of any age are unusual. Properties less than fifty years old that may qualify as exceptional include those where the entire category of the resource is fragile. The phrase "exceptional importance" does not require that the property be of national significance. It is a measure of a property's importance within the appropriate historic context, whether the scale of that context is local, state, or national.

The Santa Cruz Municipal Wharf is the last wharf of the original six that were constructed, and is the only surviving property associated with the shipping and fishing industries of the City from the early 20th century. Although, as indicated by the City of Santa Cruz website, fewer than 5 percent of the original pilings still exist at the Santa Cruz Municipal Wharf and there has been additions and alterations to the structure within the past fifty years, which has led to major losses in historic fabric and materials, it still conveys its historical significance, especially as the last surviving example of a fragile property type.

In summary, the Santa Cruz Municipal Wharf appears to be eligible to the CRHR under Criteria 1 and the City of Santa Cruz Registry under Criteria 1 and 2, and retains its historic integrity. In addition, as a property that continued to achieve significance into a period less than fifty years ago, the Santa Cruz Municipal Wharf is an exceptional example of a fragile category of properties under Criterion Consideration G. Therefore, the property is considered a historical resource for purposes of CEQA.

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*Recorded by: Jay Rehor & Corri Jimenez

*Date: 12/10/12

☒ Continuation

☐ Update

B12. Continued

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☒ Continuation

☐ Update



Early photo of Municipal Wharf, with railroad tracks at right. Original 1875 Railroad Wharf barely visible in background at left; small Pleasure Pier (part of Boardwalk) visible in background at right (Frank Perry Collection; <http://limekilnlegacies.com/pdfs/Notes-on-Santa-Cruz-Wharves.pdf>)



Ca. 1914 photo of new Municipal Wharf (left) and Railroad Wharf (right) (Santa Cruz Public Library Collection)

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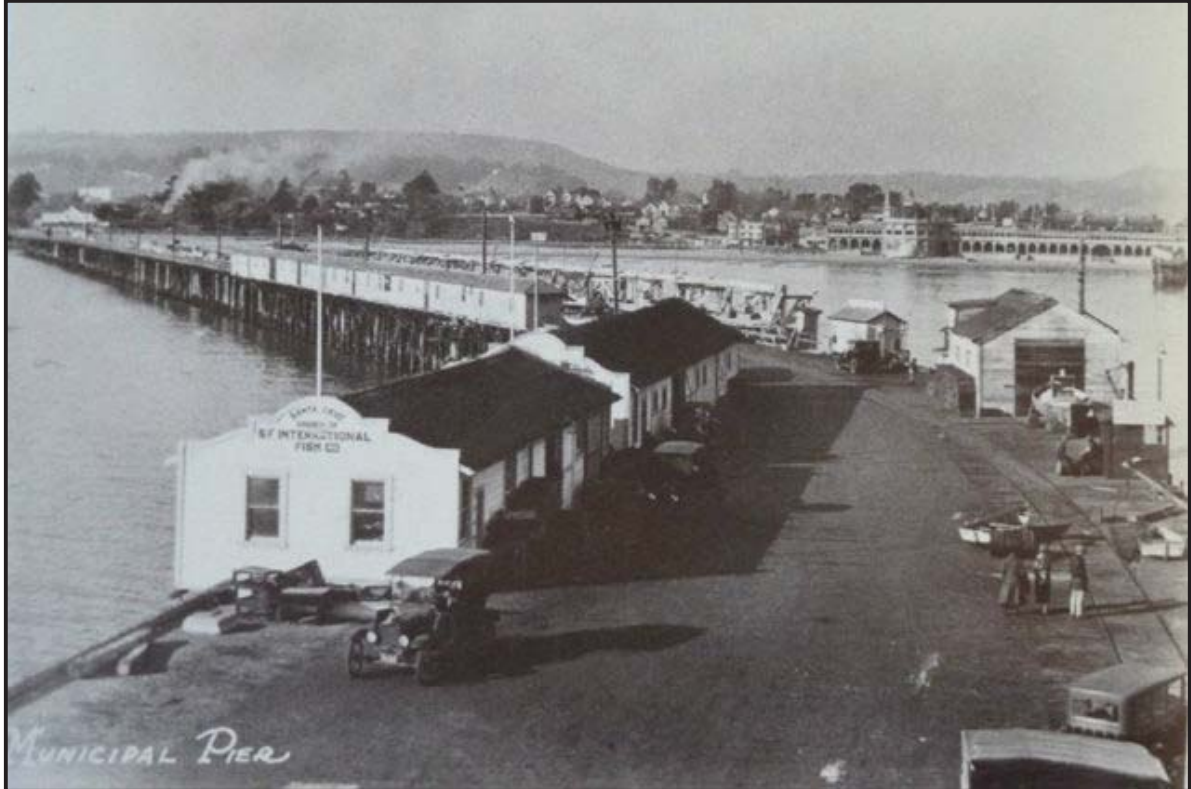
*Resource Name or # Santa Cruz Municipal Wharf

*Recorded by: Jay Rehor & Corri Jimenez

*Date: 12/10/12

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☐ Update



Ca. 1920s photo of Municipal Wharf, with railroad tracks at right. Note businesses lining west side of pier and davits on east side, similar to the present-day layout (Santa Cruz Museum of Natural History)



Ca. 1950s photo of Municipal Wharf. Railroad tracks removed; fishing fleet present on east side of pier (Santa Cruz Museum of Natural History)

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View to the East: Current view of Municipal Wharf from West Cliff Drive.



View to the West: Aerial view of Municipal Wharf in context of Santa Cruz waterfront (Beach Hill and Boardwalk in foreground; West Cliff in background).

(Photo courtesy of Robert Campbell, [http://commons.wikimedia.org/wiki/File%3AAerial view - Santa Cruz CA.jpg](http://commons.wikimedia.org/wiki/File%3AAerial_view_-_Santa_Cruz_CA.jpg))

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View to the North: Commercial Buildings, built along Wharf roadway.



View to the West: Commercial Buildings, built along Wharf roadway.

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View to the West: Commercial Buildings, built along Wharf roadway.



View to the West: Commercial Buildings, built along Wharf roadway.

**State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION**

LOCATION MAP

Primary #

HRI#

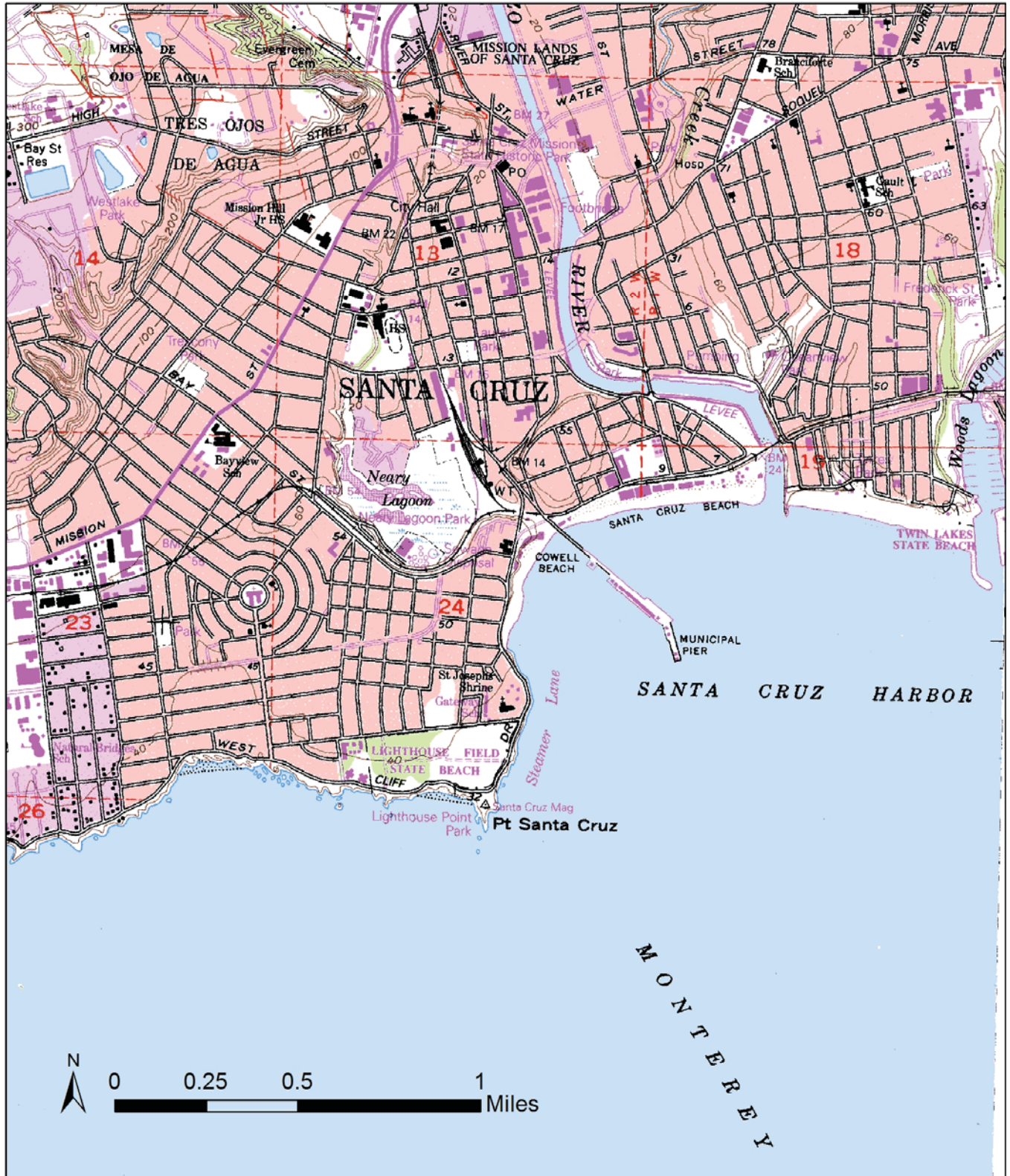
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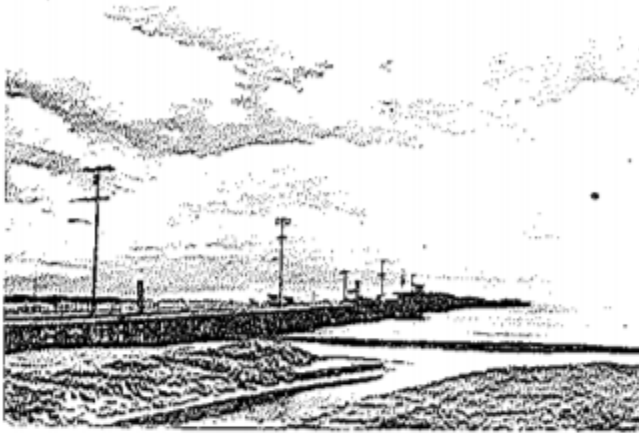
***Resource Name or #:** Santa Cruz Municipal Wharf

***Map Name:** Santa Cruz USGS 7.5 Minute Topographic

***Scale:** 1:24,000 ***Date of Map:** 1954 (P.R. 1968)



Charles Hall Page & Associates, Inc. 1976. *Santa Cruz Historic Building Survey, Vol 1.*



Foot of Washington
Municipal Wharf
1914

A typical ocean pier, originally
to accomodate shipping and now
largely devoted to restaurants
and pleasure fishing.

Excellent

DRAFT

HISTORICAL RESOURCES TECHNICAL REPORT FOR 15 MUNICIPAL WHARF, SANTA CRUZ, CALIFORNIA

Prepared for:

CITY OF SANTA CRUZ

Parks and Recreation Department

323 Church Street

Santa Cruz, California 95060

Contact: Jon Bombaci, Wharf Supervisor

Prepared by:

Kara R. Dotter, MSHP; Fallin Steffen, MPS; and Kathryn Haley, MA.

DUDEK

725 Front Street, Suite 400

Santa Cruz, California 95060

FEBRUARY 2020

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Acronyms and Abbreviations

Acronym/Abbreviation	Definition
ACHP	Advisory Council on Historic Preservation
CEQA	California Environmental Quality Act
CHRIS	California Historical Resources Information System
CRHR	California Register of Historical Resources
NHPA	National Historic Preservation Act
NRHP	National Register of Historic Places
NWIC	Northwest Information Center

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Executive Summary

The City of Santa Cruz retained Dudek to complete a historic resources technical report for 15 Municipal Wharf, Santa Cruz, California (subject property), as part of the environmental review for the Wharf Master Plan (project) in Santa Cruz, California. The Santa Cruz Municipal Wharf itself was evaluated as part of the same project, with the results presented in the *Cultural Resources Evaluation Report for the Santa Cruz Wharf Maintenance Project* completed in 2019 by Dudek.

This report includes the following components: (1) a pedestrian survey of the subject property; (2) a historical significance evaluation of 15 Municipal Wharf; and (3) an assessment of project-related impacts to historical resources in conformance with the California Environmental Quality Act (CEQA) and in consideration of all applicable local municipal code and planning documents.

Dudek conducted an intensive-level pedestrian survey of the subject property on November 15, 2019. As part of this study, Dudek evaluated the subject property under the National Register of Historic Places (NRHP) criteria, California Register of Historic Resources (CRHR), and relevant local criteria. As a result, 15 Municipal Wharf is recommended as not eligible for listing in the NRHP, CRHR, or City of Santa Cruz local register under any criteria. As such, it is not a historical resource under CEQA. No further management recommendations were required.

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1 Introduction

The City of Santa Cruz retained Dudek to complete a historical resources technical report for 15 Municipal Wharf, Santa Cruz, California (subject property), in support of the Santa Cruz Wharf Master Plan (project) (Figure 1, Project Location). The report involved a pedestrian survey of the subject property, archival and building development research, preparation of a historic context statement, and evaluation of the subject property under National Register of Historic Places (NRHP), California Register of Historic Resources (CRHR) and City of Santa Cruz local designation criteria. This study was conducted in accordance with Section 106 of the National Historic Preservation Act (NHPA) and Section 15064.5(a)(2)–(3) of the California Environmental Quality Act (CEQA) Guidelines, and the subject property was evaluated in consideration of NRHP, CRHR, and local regulations.

1.1 Project Location and Description

Project Location

The subject property was built near the northern end of the Santa Cruz Wharf. The Wharf is sited at Municipal Wharf Street in the City of Santa Cruz (City). The rectangular building rests on part of the municipal wharf. The site is bounded by Cowell Beach and Monterey Bay to the west, Beach Street to the north, Santa Cruz Beach and Monterey Bay to the east, and Monterey Bay to the south.

Project Description

The proposed project consists of adoption and implementation of the Wharf Master and construction of the two following projects recommended in the Master Plan within 2 to 5 years: Entry Gate Relocation and the East Promenade inclusive of the all access small and large boat landings. The Wharf Master Plan includes the following elements and recommendations:

1. Policies and Actions
2. Recommendations for Expansion, New Construction and Improvements
 - a. Wharf Expansion and New Facilities: The following new facilities are proposed: a new promenade on the east side of the Wharf; a new walkway on the west side of the Wharf; three new public use buildings; and two new Americans With Disabilities Act (ADA) accessible boat landings. The Master Plan also considers remodeling, infill, and intensified use of existing structures.
 - b. Structural Wharf Improvements: Recommended improvements include installation of new and replacement Wharf support piles, lateral bracing, and roadway and utility improvements, including improvements to the Wharf's pavement, drainage system, and trash collection system.
3. Circulation/Parking Circulation/Parking. Improvements are proposed to more efficiently utilize the existing circulation area, encourage alternative transportation, and relocate the Wharf entrance gates further south onto the Wharf. Other improvements include restriping of existing parking areas that would result in approximately 45-65 additional parking spaces, widening existing sidewalks for improved pedestrian access, and provision for up to 150 bicycle parking spaces.

4. Design Standards are included in the Master Plan that address building design elements, including height, materials, design, windows, roofs and displays.

The proposed improvements would expand the Wharf by approximately 2.5 acres, and as a result, sections of the Wharf devoted to public access, recreation and open space would increase from 26% to 60%. These improvements include the East Promenade, Westside Walkway and two boat facilities. Three new buildings would result in approximately 15,000 square feet of new building space for public-oriented uses; the proposed Gateway Building would result in removal of the subject building.

1.2 Project Personnel

Kara R. Dotter is a senior architectural historian and historic preservation specialist with more than 15 years' experience in historic preservation and architectural conservation. Her historic preservation experience spans all elements of cultural resources management, including project management, intensive-and reconnaissance-level field investigations, architectural history studies, and historical significance evaluations in consideration of the NRHP, CRHR, and local-level designation criteria, in addition to architectural conservation work. She is experienced managing multidisciplinary projects in the lines of land development, state and local government, and the private sector. Ms. Dotter has experience preparing environmental compliance documentation in support of projects that fall under CEQA/National Environmental Policy Act (NEPA), and Sections 106 and 110 of the National Historic Preservation Act (NHPA). She is experienced in the preparation of documentation for NRHP nominations, Historic American Building Survey (HABS), Historic American Engineering Record (HAER), and Historic American Landscape Survey (HALS). She also prepared numerous Historic Architectural Survey Reports (HASRs) and Findings of Effect (FOE) reports for the California High-Speed Rail Authority. Ms. Dotter meets the Secretary of the Interior's Professional Qualification Standards for Architectural History.

Fallin Steffen is an Architectural Historian with 4 years' experience in historic preservation, architectural conservation, and cultural resource management in the Monterey Bay Area and northern California. Ms. Steffen's professional experience encompasses a variety of projects for local agencies, private developers, and homeowners in both highly urbanized and rural areas, including reconnaissance- and intensive-level surveys, preparation of resource-appropriate and city-wide historic contexts, and historical significance evaluations in consideration of the NRHP, California Register of Historical Resources (CRHR), and local designation criteria. Ms. Steffen meets the Secretary of the Interior's Professional Qualification Standards for Architectural History. She is experienced with interdisciplinary projects spanning private and public development, transportation, and water infrastructure, and maintains experience forming educational sessions about the identification of and best practices for the preservation of historic resources.

Kathryn Haley is a senior architectural historian with over 15 years of professional experience in historic/cultural resource management. Ms. Haley has worked on a wide variety of projects involving historic research, field inventory, and site assessment conducted for compliance with Section 106, National Environmental Policy Act, and CEQA. Ms. Haley specializes in CRHR, the NRHP, evaluations of built environment resources, including water management structures (levees, canals, dams, ditches), buildings (residential, industrial, and commercial), and linear resources (railroad alignments, roads, and bridges). She specializes in managing large-scale surveys of built environment resources including historic district evaluations. She has prepared numerous Historic Resources Evaluation Reports and Historic Property Survey Reports for the California Department of Transportation. Ms. Haley also worked on the California High-Speed Rail, San Jose to Merced, and Central Valley Wye Project Sections; leading

the built environment survey, conducting property specific research, preparing the Draft Historic Architectural Survey Report (HASR) as well as co-authoring the environmental section for Cultural Resources. She meets the Secretary of the Interior's Professional Qualification Standards for historian and architectural historian. Ms. Haley has also assisted in preparation of Historic Properties Inspection Reports (condition assessments) under the direction of the Naval Facilities Engineering Command in accordance with Section 106 and Section 110 of the National Historic Preservation Act. Moreover, Ms. Haley has served as project manager, coordinator, historian, and researcher for a wide variety of project. She is also experienced in the preparation for National Register nominations, as well as, Historic American Building Survey, Historic American Engineering Record, and Historic American Landscape Survey documents.

1.3 Regulatory Setting

1.3.1 Federal

National Register of Historic Properties

The NHPA established the NRHP and the President's Advisory Council on Historic Preservation (ACHP), and provided that states may establish State Historic Preservation Officers to carry out some of the functions of the NHPA. Most significantly for federal agencies responsible for managing cultural resources, Section 106 of the NHPA directs that

[t]he head of any Federal agency having direct or indirect jurisdiction over a proposed Federal or federally assisted undertaking in any State and the head of any Federal department or independent agency having authority to license any undertaking shall, prior to the approval of the expenditure of any Federal funds on the undertaking or prior to the issuance of any license, as the case may be, take into account the effect of the undertaking on any district, site, building, structure, or object that is included in or eligible for inclusion in the NRHP.

Section 106 also affords the ACHP a reasonable opportunity to comment on the undertaking (16 U.S.C. 470f).

Title 36 of the Code of Federal Regulations, Part 800 (36 CFR 800) implements Section 106 of the NHPA. It defines the steps necessary to identify historic properties (those cultural resources listed in or eligible for listing in the NRHP), including consultation with federally recognized Native American tribes to identify resources with important cultural values; to determine whether or not they may be adversely affected by a proposed undertaking; and the process for eliminating, reducing, or mitigating the adverse effects.

The content of 36 CFR 60.4 defines criteria for determining eligibility for listing in the NRHP. The significance of cultural resources identified during an inventory must be formally evaluated for historic significance in consultation with the ACHP and the California State Historic Preservation Officer to determine if the resources are eligible for inclusion in the NRHP. Cultural resources may be considered eligible for listing if they possess integrity of location, design, setting, materials, workmanship, feeling, and association.

Regarding criteria A through D of Section 106, the quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, cultural resources, buildings, structures, and

objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association, and that (36 CFR 60.4):

- A. Are associated with events that have made a significant contribution to the broad patterns of our history; or
- B. Are associated with the lives of persons significant in our past; or
- C. Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. have yielded or may be likely to yield, information important in prehistory or history.

The 1992 amendments to the NHPA enhance the recognition of tribal governments' roles in the national historic preservation program, including adding a member of an Indian tribe or Native Hawaiian organization to the ACHP.

The NHPA amendments:

- Clarify that properties of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization may be determined eligible for inclusion in the National Register
- Reinforce the provisions of the Council's regulations that require the federal agency to consult on properties of religious and cultural importance.

The 1992 amendments also specify that the ACHP can enter into agreement with tribes that permit undertakings on tribal land and that are reviewed under tribal regulations governing Section 106. Regulations implementing the NHPA state that a federal agency must consult with any Indian tribe that attaches religious and cultural significance to historic properties that may be affected by an undertaking.

1.3.2 State

California Register of Historical Resources

In California, the term "historical resource" includes but is not limited to "any object, building, structure, site, area, place, record, or manuscript which is historically or archaeologically significant, or is significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California" (PRC Section 5020.1[j]). In 1992, the California legislature established the CRHR "to be used by state and local agencies, private groups, and citizens to identify the state's historical resources and to indicate what properties are to be protected, to the extent prudent and feasible, from substantial adverse change" (PRC Section 5024.1[a]). The criteria for listing resources on the CRHR were expressly developed to be in accordance with previously established criteria developed for listing in the NRHP, enumerated below. According to California Public Resources Code Section 5024.1(c)(1-4), a resource is considered historically significant if it (i) retains "substantial integrity," and (ii) meets at least one of the following criteria:

- 5. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage.
- 6. Is associated with the lives of persons important in our past.
- 7. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values.

1. Has yielded, or may be likely to yield, information important in prehistory or history.

In order to understand the historic importance of a resource, sufficient time must have passed to obtain a scholarly perspective on the events or individuals associated with the resource. A resource less than 50 years old may be considered for listing in the CRHR if it can be demonstrated that sufficient time has passed to understand its historical importance (see 14 CCR 4852[d][2]).

The CRHR protects cultural resources by requiring evaluations of the significance of prehistoric and historic resources. The criteria for the CRHR are nearly identical to those for the NRHP, and properties listed or formally designated as eligible for listing in the NRHP are automatically listed in the CRHR, as are the state landmarks and points of interest. The CRHR also includes properties designated under local ordinances or identified through local historical resource surveys.

California Environmental Quality Act

As described further below, the following CEQA statutes and CEQA Guidelines are of relevance to the analysis of archaeological, historic, and tribal cultural resources:

- California Public Resources Code Section 21083.2(g) defines “unique archaeological resource.”
- California Public Resources Code Section 21084.1 and CEQA Guidelines Section 15064.5(a) define “historical resources.” In addition, CEQA Guidelines Section 15064.5(b) defines the phrase “substantial adverse change in the significance of an historical resource.” It also defines the circumstances when a project would materially impair the significance of an historical resource.
- California Public Resources Code Section 21074(a) defines “tribal cultural resources.”
- California Public Resources Code Section 5097.98 and CEQA Guidelines Section 15064.5(e) set forth standards and steps to be employed following the accidental discovery of human remains in any location other than a dedicated ceremony.
- California Public Resources Code Sections 21083.2(b)–(c) and CEQA Guidelines Section 15126.4 provide information regarding the mitigation framework for archaeological and historic resources, including examples of preservation-in-place mitigation measures; preservation-in-place is the preferred manner of mitigating impacts to significant archaeological sites because it maintains the relationship between artifacts and the archaeological context and may also help avoid conflict with religious or cultural values of groups associated with the archaeological site(s).

More specifically, under CEQA, a project may have a significant effect on the environment if it may cause “a substantial adverse change in the significance of an historical resource” (PRC Section 21084.1; CEQA Guidelines Section 15064.5[b]). If a site is either listed or eligible for listing in the CRHR, or if it is included in a local register of historic resources or identified as significant in a historical resources survey (meeting the requirements of PRC Section 5024.1[q]), it is a “historical resource” and is presumed to be historically or culturally significant for purposes of CEQA (PRC Section 21084.1; CEQA Guidelines Section 15064.5[a]). The lead agency is not precluded from determining that a resource is a historical resource even if it does not fall within this presumption (PRC Section 21084.1; CEQA Guidelines Section 15064.5[a]).

A “substantial adverse change in the significance of an historical resource” reflecting a significant effect under CEQA means “physical demolition, destruction, relocation, or alteration of the resource or its immediate

surroundings such that the significance of an historical resource would be materially impaired” (CEQA Guidelines Section 15064.5(b)(1)); PRC Section 5020.1(q)). In turn, CEQA Guidelines section 15064.5(b)(2) states the significance of an historical resource is materially impaired when a project:

1. Demolishes or materially alters in an adverse manner those physical characteristics of an historical resource that convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register of Historical Resources; or
8. Demolishes or materially alters in an adverse manner those physical characteristics that account for its inclusion in a local register of historical resources pursuant to section 5020.1(k) of the Public Resources Code or its identification in an historical resources survey meeting the requirements of section 5024.1(g) of the Public Resources Code, unless the public agency reviewing the effects of the project establishes by a preponderance of evidence that the resource is not historically or culturally significant; or
2. Demolishes or materially alters in an adverse manner those physical characteristics of a historical resource that convey its historical significance and that justify its eligibility for inclusion in the California Register of Historical Resources as determined by a lead agency for purposes of CEQA.

Pursuant to these sections, the CEQA inquiry begins with evaluating whether a project site contains any “historical resources,” then evaluates whether that project will cause a substantial adverse change in the significance of a historical resource such that the resource’s historical significance is materially impaired.

If it can be demonstrated that a project will cause damage to a unique archaeological resource, the lead agency may require reasonable efforts be made to permit any or all of these resources to be preserved in place or left in an undisturbed state. To the extent that they cannot be left undisturbed, mitigation measures are required (PRC Section 21083.2(a)–(c)).

California Public Resources Code Section 21083.2(g) defines a unique archaeological resource as an archaeological artifact, object, or site about which it can be clearly demonstrated that without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

1. Contains information needed to answer important scientific research questions and that there is a demonstrable public interest in that information.
9. Has a special and particular quality such as being the oldest of its type or the best available example of its type.
2. Is directly associated with a scientifically recognized important prehistoric or historic event or person.

Impacts to non-unique archaeological resources are generally not considered a significant environmental impact (PRC section 21083.2(a); CEQA Guidelines Section 15064.5(c)(4)). However, if a non-unique archaeological resource qualifies as tribal cultural resource (PRC Section 21074(c), 21083.2(h)), further consideration of significant impacts is required. CEQA Guidelines Section 15064.5 assigns special importance to human remains and specifies procedures to be used when Native American remains are discovered. As described below, these procedures are detailed in California Public Resources Code Section 5097.98.

1.3.3 Local

City of Santa Cruz

24.12.440 Santa Cruz City Historic Building Survey

Cultural resources and landmarks in the City are under the aegis of the Planning and Community Development Department, City of Santa Cruz. The City maintains a list of Historic Landmarks, as well as other built historic resources, in the Historic Building Survey. Historic Landmark is defined in Part 5: Historic Preservation within the Community Design Chapter, as “an individual structure or other feature, or group of structures on a single lot or site, or a site having special aesthetic, cultural, architectural, or engineering interest or value of an historical nature as a ‘landmark’” (Municipal Code Section 24.12.420, amended by Ordinance No. 2003-14, effective April 22, 2003).

In order to become a Historic Landmark, or to be placed on the Historic Building Survey, a property must first be evaluated for local historic significance based on the following criteria (Municipal Code Section 24.12.440, amended by Ordinance No. 2003-14, effective April 22, 2003):

- c. The property is either a building, site, or object that is:
 - 1. Recognized as a significant example of the cultural, natural, archaeological, or built heritage of the city, state, or nation
 - 10. Associated with a significant local, state, or national event
 - 11. Associated with a person or persons who significantly contributed to the development of the city, state, or nation
 - 12. Associated with an architect, designer, or builder whose work has influenced the development of the city, state, or nation
 - 13. Recognized as possessing special aesthetic merit or value as a building with quality of architecture and that retains sufficient features showing its architectural significance
 - 14. Recognized as possessing distinctive stylistic characteristics or workmanship significant for the study of a period, method of construction, or use of native materials
 - 2. Retains sufficient integrity to accurately convey its significance

The district is:

- 15. Recognized as a geographically definable area possessing a significant concentration of buildings that are well designed and other structures, sites, and objects which are united by past events or by a plan or physical development
- 3. Recognized as an established and geographically definable neighborhood united by culture, architectural styles or physical development

Santa Cruz City Historic Districts

The City recognizes two historic districts and several potential historic districts. A City Historic District is evaluated and defined by the following criteria (Municipal Code Section 24.06.120, amended by Ordinance No. 85-05, effective 1985):

1. The proposed historic district is a geographically definable area possessing a significant concentration or continuity of sites, buildings, structures, or objects unified by past events, or aesthetically by plan or physical development
 16. The collective value of the historic district taken together may be greater than the value of each individual structure
2. The proposed designation is in conformance with the purpose of the city's historic preservation provisions, set forth in Section 24.12.400 of this title and the city's Historic Preservation Plan and the General Plan

Historic Property Zoning Incentives Ordinance

As described by the City of Santa Cruz Department of Planning and Community Development, the Historic Property Zoning Incentive Ordinance (Ord. No. 2012-19) was adopted in December 2012 to expand existing zoning variations for use by individual buildings or properties listed on the Historic Building Survey or contributing buildings or properties situated within a recognized City Historic District. The ordinance permits several Variations to Development Standards to benefit previously listed properties and incentivize owners of eligible, unlisted properties to participate in local historic preservation efforts. Additionally, these Variations help to ensure that new construction and alterations to existing historic properties within these areas conform to standards that will maintain the integrity of the City's historic landmarks, buildings, sites, objects, and contributing buildings within designated and recognized districts.

Historic Alteration and Demolition Permits

Regarding effects on federal and locally significant properties, the Santa Cruz Municipal Code states the following:

Historic Alteration Permit: The purpose of this permit is to ensure that new construction and alterations are allowed in a manner which retains the integrity of the city's historic landmarks, buildings, sites and districts over time. Administrative historic alteration permits may be approved by the zoning administrator, without a public hearing, for minor alteration projects and accessory structures. Historic alteration permits may be approved by the city historic preservation commission, after a public hearing, for non-minor alteration projects. Such a permit is required before any person shall carry out or cause to be carried out, on the site of a designated landmark, or on the site of a building listed in the City of Santa Cruz Historic Building Survey, or on the site of a structure in an historic overlay district, any material change in exterior appearance of any such site or structure through alteration, construction or relocation. This section of the Zoning Ordinance is also part of the Local Coastal Implementation Plan (Section 24.08.900).

Historic Demolition Permit: The purpose of this permit is to ensure that no person shall demolish or cause to be demolished any building listed on the Santa Cruz Historic Building Survey, any designated historic landmark or any building in an historic overlay district without approval of an historic demolition permit (Section 24.08.1000)

City of Santa Cruz General Plan 2030

The city of Santa Cruz General Plan 2030 outlines policies to ensure that archeological, paleontological and built environment resources are safeguarded from the impacts of development within the City by establishing a clear framework for updating cultural sensitivity maps, inventories and zoning ordinances. The document also identifies a third type of cultural resource: historic businesses and enterprises present in the City, called a Traditional Cultural Property, or "TCP".

City of Santa Cruz Local Coastal Program, 1994-2005

The City of Santa Cruz Local Coastal Plan, 1994-2005 includes a Cultural Resources Element that outlines the policies and programs to ensure the proper treatment of cultural resources located near the coastal areas of the City.

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Figure 1. Project location and vicinity map

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2 Background Research

2.1 Building Development and Archival Research

Historical Newspaper Review

Dudek reviewed historical newspapers from Santa Cruz covering the area surrounding the Municipal wharf in an effort to understand the development of the historic property. These documents were used in the preparation of Section 3 of this report.

Historical Aerial Photographs

A review of historical aerial photographs was conducted as part of the archival research effort for the proposed project from the following years: 1931, 1948, 1952, 1956, 1964, 1968, 1982, 1987, 1993, 1998, 2001, 2005, 2009, 2010, 2012, 2014, and 2016.

Table 1. Historical Aerial Photograph Review

Year	Observations and Findings
1931	The Wharf is extant by this time and presents as a long narrow structure projecting southeastwardly into the bay with a small toe segment oriented due south. A large L-shaped building is present on the beach to the west of the Wharf entrance beside what appears to be a pipeline or underwater transmission line disappearing into the water. Several cars are visible parked in three rows of angled parking of the north of this building. The long southeast section of the Wharf contains two narrow rectangular structures with gabled roofs while the toe end contains one long, narrow, gable-roofed building on the western edge and a large rectangular building with raised end parapets at the central end of the wharf. The northeastern edge of the Wharf is lined with davits to lower and raise small crafts from the ocean onto the Wharf platform.
1940	Little change to the subject property is noted, however another building is now present on the eastern side of the Wharf entrance.
1948	Appearance of the Wharf is consistent with the previous photograph. The east Wharf entrance building has multiplied into a small complex of three rectangular buildings.
1952	Appearance of the Wharf is mostly consistent with previous photograph.
1956	Appearance of the Wharf is consistent with previous photograph. The area surrounding the Wharf entrance has been defined by the intersection of multiple paved roads.
1964	The large parapeted building at toe of the Wharf has been replaced with a small square building (presently a public restroom) and a larger rectangular one (presently the Dolphin Restaurant). Eight square viewing ports cut through the decking of the Wharf and offer a view to the piles beneath. Square footage has been added to the western side of the Wharf toe providing parking spaces at the end of structure, the surface of which is now entirely paved. The area between and the buildings along the southeast section of the Wharf appear to be enlarged towards the southeast off of the Wharf since the prior aerial photograph. The building on the western beach of the Wharf entrance has also been demolished by this time and a new Hotel (presently the Dream Inn) stands further to the west.
1968	A northeastern section of the Wharf has been expanded to provide more parking space on the Wharf. Two of the long narrow buildings have again expanded towards the southeast.

Table 1. Historical Aerial Photograph Review

Year	Observations and Findings
1982	The Wharf square footage has been further expanded in several areas and now fills the current footprint of the Wharf, save for a small section nested in the crook leading to the Wharf toe.
1987	The image quality is poor, so certain details are obscured. The footprint of the Wharf now appears in its present configuration.
1993	The buildings which now line the southeastern area of the Wharf are those present today.
1998-2016	No discernable changes.

Historical Sanborn Map Review

A review of historical Sanborn Map Company fire insurance maps covering the City of Santa Cruz was conducted as part of the archival research effort for the proposed project. The 1928 and 1928-1950* editions of the Santa Cruz map included the Municipal Wharf and are described in more detail below.

Table 2. Historical Sanborn Map Review

Year	Observations and Findings
1928	The Wharf is extant by this time and presents as a long narrow structure projecting southeastwardly into the bay with a small toe segment oriented due south. A Southern Pacific rail spur runs the length of the wharf and terminates on the northeastern side of the large rectangular warehouse building at the toe-end of the wharf. An irregular L-shaped restaurant building is present on the beach to the west of the wharf entrance and a smaller bathhouse to the west of that. The long southeast section of the wharf contains two narrow rectangular structures marked as 'storage and storerooms', while the toe end contains one long, narrow 'fish market' building on the western side. The subject property is not extant (Figure 3).
1928-1950*	Multiple combination restaurant/stores are now present in the three buildings that now surround the entrance onto the wharf at the termination of Front Street. The narrow buildings on the southwestern side of the wharf appear to now be continuous and contain multiple stores and restaurants, as well as a welding shop. The fish market on the toe of the wharf is still marked as such, but it has been joined by two small buildings in the elbow of the wharf across from a gas and oil station. The wharf has been widened by this time. The warehouse at the end of the wharf now contains a small store and an office. The subject property is not extant (Figure 4).

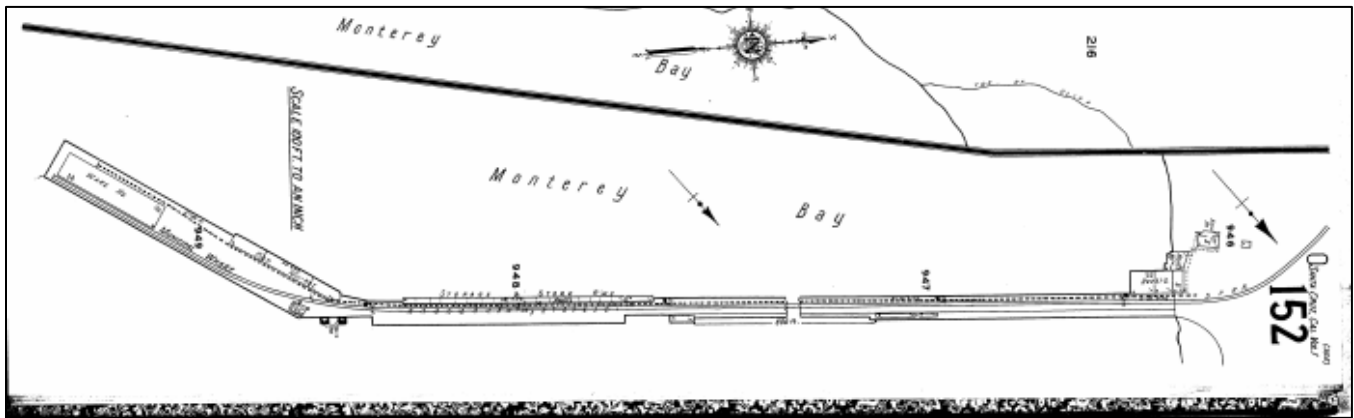


Figure 2. The Santa Cruz Municipal Wharf as it appeared in the 1928 City of Santa Cruz Sanborn Company fire insurance map (Sanborn 1928:152).

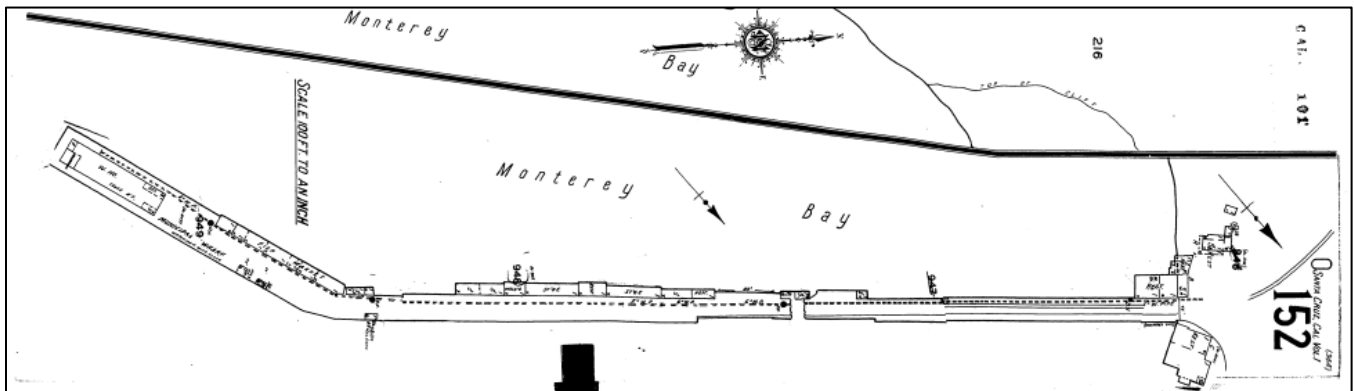


Figure 3. The Santa Cruz Municipal Wharf as it appeared in 1950 City of Santa Cruz Sanborn Company fire insurance map (Sanborn 1928-1950*: 152).

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3 Historical Context

The following historic context addresses relevant themes concerning the history of the subject property. It begins with an overview of the development of Santa Cruz County and the City of Santa Cruz, and concludes with a discussion of the historical development of the Santa Cruz Municipal Wharf and the subject property.

3.1 Historical Overview of the City of Santa Cruz

Spanish Period (1769–1822)

The earliest known European exploration of the Monterey Bay was a Spanish envoy mission led by Sebastián Vizcaíno in 1602. The purpose of the voyage was to survey the California coastline to locate feasible ports for shipping, and Vizcaíno had explicit instructions prohibiting the creation of settlements and interacting with local Native Americans. Finding the bay to be commodious, fertile, and extremely favorable for anchorage between Manila and Acapulco, Vizcaíno named the Bay “Monterey” after the Conde de Monterey, the present Viceroy in Mexico (Chapman 1920; Rensch et al. 2002).

Despite being mapped as an advantageous berth for Spanish shipping efforts, the epicenter of Spanish settlement in Alta California did not make its way to the Monterey Bay until the second half of the 18th century. In an effort to prevent the establishment of English and Russian colonies in northern Alta California, Don Gaspar de Portolá, the Governor of Baja, embarked on a voyage in 1769 to establish military and religious control over the area. This overland expedition by Portolá marks the beginning of California’s Historic period, occurring just after King Carlos III of Spain installed the Franciscan Order to direct religious colonization in assigned territories of the Americas (Rensch et al. 2002; Lehmann 2000; Koch 1973).

On their quest to locate the Monterey Bay from the 160-year-old accounts of Sebastián Vizcaíno, the Portolá expedition first reached the present-day territory of Santa Cruz on October 17, 1769. On this day, the Catholic calendar celebrates Saint Lawrence, so the large river the party camped alongside was deemed the “San Lorenzo.” After mistakenly circumventing the Monterey Bay and reaching the San Francisco Bay, the expedition backtracked to San Diego. The following year on May 31, 1770, a second expedition was organized by Portolá, resulting in the successful location of the Monterey Bay. However, it would be an additional 21 years before the Franciscan order would establish Mission Santa Cruz in the area near the San Lorenzo River (Koch 1973).

Father Fermín Lasuén, Corporal Luis Peralta, and five soldiers established Mission Santa Cruz on August 28, 1791, as the 12th mission in the California Mission system. Native Americans were forced to build the mission church and auxiliary structures from local timber, limestone, and adobe, as well as to cultivate wheat, barley, beans, corn, and lentils for the mission padres and soldiers. The forced conversion of the local native population proved continuously problematic as Mission Santa Cruz was overwhelmed by rebellions, pestilence, and building failure. The land taken by the Spanish was eventually repatriated to the Native tribes, but the massive decline in the population as a result of disease and abandonment meant that by the time of this decree, few eligible recipients remained alive and in the area (Lehmann 2000; Koch 1973).

The expansion of Spanish control in the Santa Cruz region was not limited to the development of religious infrastructure. In 1795, Spain called for the establishment of three self-governing Pueblos in Alta California that would remain free from military and religious oversight. Villa de Branciforte was established in 1797 on the opposite bank of the San Lorenzo River from Mission Santa Cruz. The 40 initial settlers were not provided with the resources promised to them by Spain to build housing or cultivate the land, but instead made due with crude dwellings of their own design. Despite the Villa de Branciforte project being deemed a failure almost immediately by Spain, the population grew quickly in the initial years. By 1803, there were 107 enlisted inhabitants in Villa de Branciforte. However, the population mainly derived from former soldiers, artisans, and criminals who lacked the apposite skill to farm and sustain themselves. By 1817, the population dwindled back to 52 as people followed new opportunities (Lehmann 2000; Koch 1973).

Mexican Period (1822–1848)

After more than a decade of intermittent rebellion and warfare, New Spain (Mexico and the California territory) won independence from Spain in 1821. In 1822, the Mexican legislative body in California ended isolationist policies designed to protect the Spanish monopoly on trade, and decreed California ports open to foreign merchants (Cleland 2005; Dallas 1955).

Extensive land grants were established in the interior during the Mexican Period, in part to increase the population inland from the more settled coastal areas where the Spanish had first concentrated its colonization efforts. Land grants to citizens covered over 150,000 acres of present-day Santa Cruz County, including two that overlapped with the present Westside neighborhood of Santa Cruz: *Potrero y Rincon de San Pedro de Reglado* (1843) and *Tres Ojos De Agua* (1844). The subject property, however, is situated on land that was associated with Mission Santa Cruz and was never formally distributed under this program (Lehmann 2000; Koch 1973: 11; Robinson 2012).

American Period (1848–Present)

The Mexican–American War ended with the Treaty of Guadalupe Hidalgo in 1848, ushering California into its American Period. Santa Cruz was designated as one of the 27 original counties of California on February 18, 1850, shortly before California officially became a state with the Compromise of 1850 that also designated Utah and New Mexico (with present-day Arizona) as U.S. territories. The new state of California recognized the ownership of lands in the state distributed under the Mexican land grants of the previous several decades (Cleland 2005; Waugh 2003; Koch 1973).

As the Gold Rush was picking up steam in 1849, a massive influx of people seeking gold steadily flooded the rural counties of California. Insightful entrepreneurs in Santa Cruz also saw the arrival of opportunity-seeking laborers as a means to harvest the abundant natural resources found throughout the area. The lumber, fishing, hydraulic lime, cement, and leisure industries formed the economic foundation of the County of Santa Cruz, while in the fertile acreage of central and south Santa Cruz County agriculture took hold as the leading economic venture (Lehmann 2000; Koch 1973).

Santa Cruz incorporated as a town in 1866 under the recent laws of the new state of California, and in 1876, the town received its first charter and became a city. The charter outlined the City's leadership as a Mayor and a Common Council of four members (MAH 2019).

Development of the Shipping Industry and Wharves in Santa Cruz

In order to transport the array of raw natural resources available in Santa Cruz to a wider consumer market, a series of five wharves were constructed along the Santa Cruz shoreline between 1849 and 1914. Susan Lehman succinctly summarizes wharf development in the City of Santa Cruz Historic Context Statement (Lehman 2000:27):

Before railroads and motor roads linked the community to the outside world, the city's industries were dependent on shipping. Getting goods to waiting ships was the first problem and the initial solution was primitive at best. Lumber was dragged through the surf and hoisted aboard waiting schooners. Men carried lighter objects on their shoulders to small boats which in turn brought them to ships. Elihu Anthony, a merchant and foundry owner came up with a marginally better system with his partner Edwin S. Penfield when in 1847 they built a rudimentary wharf at the foot of the present Bay street. The "wharf" was really a plank chute steep enough to slide a sack of potatoes into a waiting rowboat. It was later bought by Davis and Jordan who owned a small fleet of schooners that carried shipments of lime from their kilns. They replaced the structure in 1856 with a 1000 foot wharf and developed a system wherein the lime was transported by tram cars moved by gravity down an incline, then hauled with horses back up the hill. This wharf was ultimately destroyed by heavy seas in 1907.

Lehman's account, as well as multiple earlier sources, related to the development of the second wharf completed in Santa Cruz, the Gharkey Wharf (sometimes also called the "Powder Mill Wharf" or "Steamship Wharf" (Perry et al. 2012, 1), indicate that it was later converted into the Railroad Wharf. However, a more recent review of this history authored by local historians Frank Perry, Barry Brown, Rick Hyman, and Stanley D. Stevens asserts that although the relationship between the two wharves has been reported in this manner in the past, this progression is not accurate. In relation to the development of the Gharkey and Railroad wharves, *Notes on the History of Wharves at Santa Cruz, California* states that (Perry et al. 2012: 2):

The wharf built at the foot of Main Street by David Gharky in 1857 was mostly acquired by the California Powder Works in 1865 and was demolished in 1882. The Santa Cruz and Felton Railroad had a new wharf built at the foot of Pacific Avenue in 1875. It was commonly known as the Railroad Wharf because it was fitted with tracks to accommodate railroad cars. This wharf stood until 1922.

The Railroad Wharf was entirely new and not a conversion of the Gharky Wharf. In 1877, however, an S-shaped Connecting Wharf with railroad tracks was built between the two wharves. This enabled the railroad to ship from the former Gharky Wharf (then called the Powder Mill Wharf) as well as from the Railroad Wharf. The connection eliminated the dangerous practice of hauling explosives through town by wagon and storing them on the Powder Mill Wharf while waiting to be shipped.

Lehman summarizes the fate of the railroad wharf as follows: "The city's fishing fleet used the railroad wharf as its base of operations but moved to the municipal wharf when that was constructed by the city in 1914. The railroad wharf was finally torn down in 1922 (Lehman 2000: 27).

Santa Cruz as a Tourist Destination

Another major industry emerged in Santa Cruz around the mid-19th century that would help to develop areas adjacent to the city: beach tourism. Interest in the beauty of the area drew visitors to Santa Cruz as early as the 1860s. The arrival of the railroad to Santa Cruz allowed for greater mobility to the area, by both residents and tourists alike, and largely alleviated Santa Cruz's dependency on shipped goods arriving by sea. A rail line running from Gilroy to Santa Cruz by way of Watsonville was completed by 1876, followed shortly thereafter by a narrow gauge line from Santa Cruz to Felton. As the port altogether declined due to lack of use and the ease of transport by train, the beachfront areas of the city presented savvy entrepreneurs with emerging opportunities (Lehmann 2000).

By 1893, Harper's Weekly acknowledged Santa Cruz as a beach destination, promoting beachside institutions like the Neptune Baths built in 1884 by Captain C.F. Miller, and giving the small coastal city the push it needed to become a national tourist destination. The economic transition away from the early industries of Santa Cruz towards tourism during this period helped to alleviate the strain placed on the fisheries and forests in the County (Lehmann 2000).

Lehman (2000:27) briefly describes the Pleasure Pier, which was the previous pier or wharf structure constructed in 1904 prior to the development of the Santa Cruz Municipal Wharf in 1914:

Although it only exists in photographs, the Pleasure Pier, constructed in 1904 as part of the development of [the] first Casino and boardwalk, was long a part of the city's waterfront attractions. It was torn down in 1962 at the same time the boardwalk's plunge was converted into a miniature golf course.

3.2 Development of the Santa Cruz Municipal Wharf

The completion of the most recent pier in Santa Cruz occurred in December 1914 (Figure 4). The Santa Cruz Municipal Wharf represented a fusion between the shipping and tourism industries within Santa Cruz because "in contrast to the city's early wharves that had warehouses and businesses associated with fishing and shipping, the current wharf [was intended to be] tourist oriented with restaurants, stores and souvenir shops" (Lehman 2000: 27).

The following is a brief history of the wharf excerpted from a report by Bridgette Maley of architecture + history, LLC as part of the Santa Cruz Wharf Master Plan project (Maley 2016: 3-5):

The Santa Cruz Wharf was constructed in 1914, as the last in a series of six piers that were constructed on the Santa Cruz waterfront between 1849 [1847] and 1914. The Wharf was originally envisioned as a commercial enterprise, built with public funds to further the economic development of the City. Soon after its construction, the Wharf became an attractive facility for the mooring and off-loading of commercial fishing vessels.

Since its construction in 1914, the Wharf has expanded from approximately and widened from its original 100-foot width over the years, but the location and length of the Wharf remain unchanged. Since its construction in 1914, the Wharf has expanded from approximately 4.2 acres to 7.5 acres. The Wharf increased by 3.3 acres between the 1950s and the 1980s for commercial uses and parking.



Figure 4. Santa Cruz Municipal Wharf on opening day in 1914 looking towards the present site of the Boardwalk. At the far left the Old Railroad Wharf is just visible and at the right the Pleasure Pier (Koch Collection, Santa Cruz Museum of Art and History)

Davits for lifting fishing vessels into and out of the ocean once lined the Municipal Wharf, as did a rail line, warehouses, and fishing-related storefronts. The rail line was taken out in the late 1940s to early 1950s, and replaced with a paved deck for vehicle traffic.

Over the years, the Wharf has evolved in role, function and identity. From its initial role as a cargo handling and shipping pier to its later adaptation to serve the commercial fishing industry, the early decades of the Wharf were closely tied to the resources of Monterey Bay. After World War II and beginning in the 1950s, the Wharf was significantly expanded for commercial uses and parking. The commercial uses were initially a direct outgrowth of the commercial fishing industry, incorporating fish sales and featuring prepared seafood dishes in an open air setting in close conjunction with off-loading and handling of the daily catch.

Many of the Wharf's original buildings and structures have been demolished or greatly altered, including a large warehouse building that was located at the bayward end of the Wharf. According to the Wharf Master Plan, this structure is significant from a historic point of view because "it gave

physical expression to the environmental conditions that made deepwater maritime functions possible and that contributed to the configuration of the end of the Wharf to optimize berthing relative to wind and wave conditions.” Today, the largely industrial and commercial oriented historic-era businesses of the Municipal Wharf have been replaced by restaurants, retail, and other tourist oriented services. Over 20 buildings and structures associated with these businesses are located on the deck of the Wharf, as well as other built environment features associated with utilities and lighting.

The Historic Context Statement for the City of Santa Cruz (Lehmann, 2000) indicates that within the context Santa Cruz’s economic development between 1850 and 1950, the Municipal Wharf represents an important property type related to industrial development and transportation. The wharves and piers first built in the early days of Santa Cruz’s history evolved with the changes in the City’s economy. The shipping wharves gave way to a railroad wharf and finally to a fishing and commercial wharf that was constructed in 1914 to serve the City’s needs. The Wharf has been rebuilt and refurbished over time and the warehouses and commercial fishing boats have been replaced by restaurants and concessions for sports fishing and sightseeing. The circa 1920s fishing boat, the Marcella, is on display at the north wharf, and, as stated by Lehmann, may be the “last remnant of the time when the waterfront served the needs of a booming industrial economy.”

As the last of a series of six piers constructed to serve industrial and commercial development in Santa Cruz, and which have been a defining element of the Santa Cruz waterfront since 1849, the Santa Cruz Municipal Wharf serves as a vital physical reminder of that history. The historical fishing industry was also heavily dependent on the wharves of Santa Cruz, including the Municipal Wharf, prior to 1950.

The Wharf no longer serves the commercial fishing industry. Currently, the Wharf is one of a number of destination attractions in Santa Cruz’s Beach Area. The City estimates that approximately 2.5 million visitors currently come to the Wharf annually. Although the Wharf provides opportunities for pier fishing, as well as kayak and small fishing boat rentals, the Wharf’s identity is primarily related to the commercial uses along its length.

Measuring 2,745 feet in total length, the Wharf presently constitutes the longest example of a wooden pier structure in California. It is also the longest pier of any kind located on the Pacific Ocean coastline (See California 2020).

Maley also describes the physical construction of the wharf in the context of other, similar structures (Maley 2016: 3):

The Wharf is a timber pier, entirely constructed of wood and supported by approximately 4,445 Douglas fir piles. The Wharf is the longest timber piles supported pier structure in the United States and one of the longest in the world. Called a wharf because of its early function in off-loading cargo, the Santa Cruz Wharf is actually a pier structure that extends to deep water, historically facilitating the mooring of large vessels, unlike a wharf which typically runs parallel to the shore.

Specifically, the Wharf piles are Douglas fir measuring 16 inches in diameter. The piles are along driven rows, or “bents,” spaced on-center at approximately 15 feet apart. The piles and bents are spanned by 6” x 12” and 4” x 12” beams, also known as “stringers.” The piles are each numbered, starting from the farthest point and going

west to the shore. The Wharf deck features two inches of asphalt on walkways and roads (Maley 2016: 3; Bombaci Pers. Comm.).

After initial construction in 1914, the wharf underwent a series of additions and alterations. Additions to the narrow footprint of the wharf occurred on both sides during construction campaigns in the 1950s, during 1968-1972, during 1970-1980s, in 1982, and in 2008 (Figure 5).

The subject property located at 15 Municipal Wharf was designed by the City of Santa Cruz Department of Engineering in early 1958, during the 1950s period of expansion to the wharf. Constructed on a purpose-built addition to the original pier, the building provided a new, larger facility for the boat rental operations, which were originally located directly opposite the subject property.

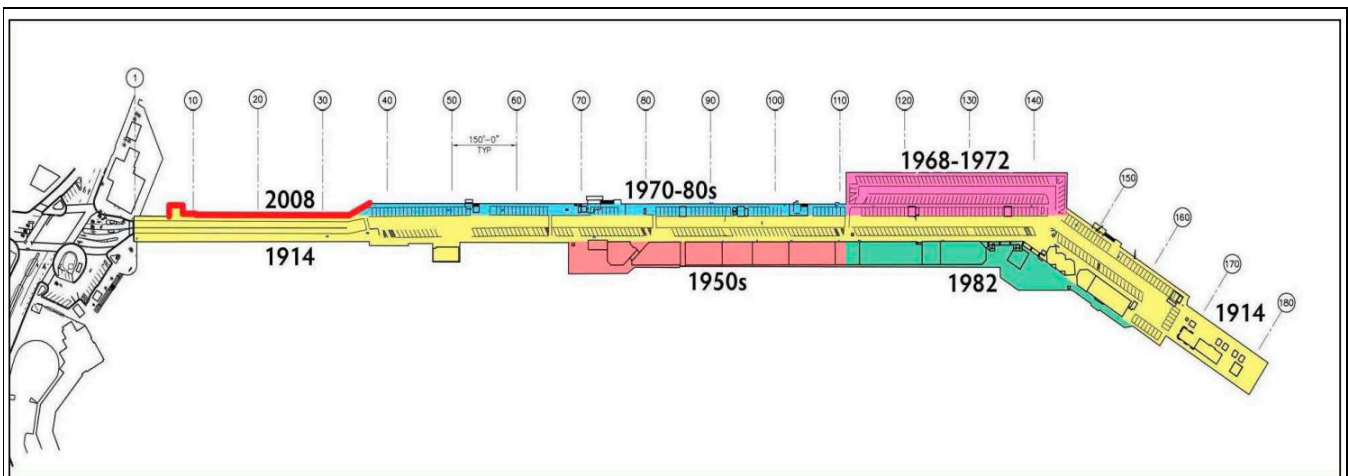


Figure 5. Graphic representation Santa Cruz Municipal Wharf's development over time.

3.3 Architectural Style: Contemporary

The subject building was constructed in the Contemporary architectural style which was popular c. 1945-1990, dominating avant-garde domestic architecture in the 1960s. Contemporary buildings drew inspiration from Frank Lloyd Wright's Usonian houses and his writing in shelter magazines and professional journals. Character-defining features of the Contemporary style include:

- Low-pitched gable roofs with wind overhanging eaves
- Exposed roof beams
- Windows in gable ends or just below the roofline on non-gabled elevations
- Extensive use of natural materials, including wood, stone, brick, and occasionally concrete
- Broad expanses of uninterrupted wall surfaces, especially on the main elevation
- Asymmetrical shape
- Typically with a recessed or obscured entry door

- Front-gabled subtypes include a broad one-story form with a very low-pitched roof that was popularized by Joseph Eichler (1900-1974), a California developer

4 Historic Resources Survey

Dudek conducted an intensive-level pedestrian survey of the subject property on November 15, 2019. The survey entailed taking photographs of the subject property, documenting its existing condition, and taking notes. These documents are on file at Dudek's Santa Cruz office.

4.1 Description of 15 Municipal Wharf

The one-story commercial building was built during 1958 in the Contemporary architectural style, with exterior walls clad in painted stucco or painted T1-11 plywood siding and the roof clad in composite shingles. The building is rectangular in plan, with a low-pitched front-gable roof, wide overhanging eaves, and exposed roof beams. The roof extends northward over a space open on the main (northeast) elevation, similar in design to a car port and currently used for storage. The enclosed part of the building is bilaterally symmetrical, with a central double-leaf full-lite entry door flanked on either side by a large rectangular fixed window and a smaller rectangular fixed window, both tilted forward, with a series of quadrilateral-shaped fixed windows in the gable end above. Two decorative murals are painted onto the stucco cladding at each end of the main elevation, adjacent to the building's corners (Figure 6).

The northwest elevation presents as two sections (Figure 7). Approximately two-thirds of the width, representing the side of the carport-like area, comprises four vertical wood timbers supporting four roof beams oriented end-on that then support the shingle-clad roof. The bottom of the wall appears to be clad in stucco, while the upper half is open to the elements. Two of the open sections are covered with a grid of sturdy metal wires, and the third open section was recently covered with T1-11 plywood siding. The remaining third of the elevation is recessed approximately 10 feet and presents as a monolithic stucco-clad wall.

The southeast elevation also presents as two sections (Figure 8). The first section is recessed approximately 6 feet, representing the rear of the carport-like portion, and was recently clad with T1-11 plywood siding. The second section, corresponding to the enclosed portion of the building, displays bilateral symmetry, with two fixed round windows protected by a grid of sturdy metal wires and six quadrilateral-shaped fixed windows above on the gable end. The wall surface between the quadrilateral windows is clad with T1-11 plywood siding. Wide overhanging eaves supported by exposed roof beams shelter the gable-end windows below.

The southeast elevation abuts the adjacent building, leaving only approximately three feet of stucco-clad wall exposed adjacent to the main elevation (Figure 9).

Observed Alterations:

- Enclosure of open carport-like area (c. 2016-2019)
- Removal of eaves along southeast elevation and construction of new building against exterior wall; subject property no longer free-standing (c. 1968-1982)
- Two painted murals on main elevation (unknown date)
- Attachment of metal grid to round windows (unknown date)
- Removal of and alterations to the architect's original interior design (unknown date)

HISTORICAL RESOURCES TECHNICAL REPORT FOR
15 MUNICIPAL WHARF, SANTA CRUZ, CALIFORNIA

- Extension of original 1914 pier to accommodate the existing building (c. 1957)
- Second extension of pier off the rear and northwest elevations (c. 1982)



Figure 6. Main (northeast) elevation, view looking southwest (IMG_0634)



Figure 7. Northwest elevation, view looking southeast (IMG_0595)



Figure 8. Rear (southwest) elevation, view looking east (IMG_0598)



Figure 9. Southeast elevation, view looking west (IMG_0619)

5 Significance Evaluation

The Santa Cruz Wharf was found significant for listing in the CRHR under Criterion 1, and under Santa Cruz Criteria. The Santa Cruz Wharf also appears eligible for listing in the NRHP under Criterion A for its continued associations with the industrial development, transportation, commerce, and recreation development of the City. Additionally, it appears eligible under NRHP Criterion C as a distinctive engineering structure known to be the longest wooden pier structure located along the United States Coast of the Pacific Ocean (Dudek 2019). The character-defining features associated with the Santa Cruz Wharf are its location, setting, alignment, wooden materials (piers), its original length of 2,745 feet long and its continued function as a wharf structure. The contemporary infrastructural elements on the wharf deck, including roads, lighting, utilities, and buildings, do not date to the 1914 period of significance identified for the Santa Cruz Wharf. As such, they are considered non-contributing elements of the structure.

As noted above, the buildings located on the Wharf are not contributing elements of the historical resource. The purpose of the following evaluation is to assess the property's historical significance and integrity as an individual property. The building located at 15 Municipal Wharf was recorded and evaluated in consideration of NRHP, CRHR, and City of Santa Cruz designation criteria and integrity requirements. The property significance evaluation was prepared by Dudek architectural historian Kara R. Dotter, MSHP, who meets the Secretary of the Interior's Professional Qualification Standards for architectural history. Completed DPR 523 forms are located in Appendix A.

5.1 NRHP/CRHR Statement of Significance

Criterion A/1: That are associated with events that have made a significant contribution to the broad patterns of our history.

The subject property is a late addition to the Santa Cruz Municipal Wharf, providing a new building to house the boat rental operation that was originally located on the opposite side of the wharf. The Santa Cruz Municipal Wharf itself is significant as the last built representative of the important function that shipping played in the early economic growth and the sustained development of the City of Santa Cruz. However, the subject property merely rests upon the wharf structure. Additionally, it is not the original home of the boat rental operation, nor does the function it serves associate it with the shipping industry and the part it played in the early economic growth and sustained development of the City of Santa Cruz. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion A/1.

Criterion B/2: That are associated with the lives of persons significant in our past.

To be found eligible under B/2 the property has to be directly tied to an important person and the place where that individual conducted or produced the work for which he or she is known. Archival research failed to indicate any direct association of 15 Municipal Wharf with individuals known to be historic figures at the national, state, or local level. As such, the subject property is not known to have any historical associations with people important to the nation's or state's past. Due to a lack of identified significant associations with important persons in history, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion B/2.

Criterion C/3: That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction.

The subject property is a modest, unremarkable representation of the Contemporary architectural style. It retains several features indicative of the Contemporary architectural style, including low-pitched gable roofs with wide overhanging eaves, exposed roof beams, windows in gable ends or just below the roofline on non-gabled elevations, extensive use of natural materials, broad expanses of uninterrupted wall surfaces, an overall asymmetrical shape, and as a front-gabled subtype it includes a broad one-story form with a very low-pitched roof popularized by Joseph Eichler (1900-1974), a California developer. Despite retention of such design features, the building has underwent several alterations, including enclosure of the open carport-like area; removal of eaves along southeast elevation and construction of a new building against the exterior wall, meaning the subject property is no longer free-standing; two painted murals on main elevation; attachment of metal grid to round windows; and removal of and alterations to the architect's original interior design. Additionally, the building does not represent the work of a master, it does not possess high artistic values, nor does it represent a significant and distinguishable entity whose components may lack individual distinction. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion C/3.

Criterion D/4: That have yielded, or may be likely to yield, information important in prehistory or history.

There is no evidence to suggest that the subject property has the potential to yield information important to state or local history. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion D/4.

5.2 City of Santa Cruz Statement of Significance

City of Santa Cruz Criteria 1-6 are similar to the four criteria for listing on the NRHP or CRHR. As such, for all the reasons discussed above, 15 Municipal Wharf is recommended as not eligible for listing under City of Santa Cruz Criteria 1-6. Regarding Criteria 7, the numerous additions to and alterations of the subject property as originally designed diminish its integrity of location, design, workmanship, materials, and feeling. Integrity of setting is lost due to the alteration from its original condition as a free-standing building with limited access on the west and south elevations. Integrity of association remains, as it still serves its original function as a boat rental facility, retaining its ties to the tourism industry centered on the wharf. As such, the subject property does not retain sufficient integrity to accurately convey its significance. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under any City of Santa Cruz criteria.

5.3 Integrity Discussion

In addition to meeting one or more of the above criteria, an eligible resource must retain integrity, which is expressed in seven aspects: location, design, setting, workmanship, materials, feeling, and association. All properties change over the course of time. Consequently, it is not necessary for a property to retain all its historic physical features or characteristics. The property must retain, however, the essential physical features that enable it to convey its historic identity. The essential physical features are those features that define both why a property is significant and when it was significant.

In the case of 15 Municipal Wharf, the numerous additions to and alterations of the subject property as originally designed diminish its integrity of location, design, workmanship, materials, and feeling. Integrity of setting is lost due to the alteration from its original condition as a free-standing building with limited access on the west and south elevations. Integrity of association remains, as it still serves its original function as a boat rental facility, retaining its ties to the tourism industry centered on the wharf. As such, the subject property does not retain sufficient integrity to accurately convey its significance.

5.4 Summary of Evaluation Findings

As a result of research, documentation, and evaluation of the subject property, 15 Municipal Wharf is recommended as not eligible for listing under any NRHP, CRHR, or City of Santa Cruz designation criteria. Additionally, the subject property does not retain sufficient integrity for listing. The subject property was evaluated in accordance with Section 15064.5(a) (2)–(3) of the CEQA Guidelines using the criteria outlined in Section 5024.1 of the California Resources Code, and for the reasons detailed above it does not constitute a historical resource for the purposes of CEQA.

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6 Findings and Conclusions

As a result of Dudek's research, site survey, and property significance evaluation, the following section presents a summary of eligibility conclusions for 15 Municipal Wharf.

6.1 Summary of Findings

As stated above, 15 Municipal Wharf is recommended as not eligible for listing under any NRHP, CRHR, or City of Santa Cruz designation criteria, nor does the subject property retain sufficient integrity for listing. Additionally, the subject property does not constitute a historical resource for the purposes of CEQA. As such, there would be no impact to historical resources and no management recommendations are required.

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Appendix A

DPR forms for 15 Municipal Wharf

State of California & The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary #
HRI #
Trinomial
NRHP Status Code

Other Listings
Review Code

Reviewer

Date

Page 1 of 15 *Resource Name or #: (Assigned by recorder) 15 Municipal Wharf

P1. Other Identifier: _____

*P2. Location: ☐ Not for Publication ☒ Unrestricted

*a. County Santa Cruz and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Santa Cruz Date 1954 (revised 1994) T ; R ; of of Sec ; B.M.

c. Address 15 Municipal Wharf City Santa Cruz Zip 95060

d. UTM: (Give more than one for large and/or linear resources) Zone 10S, 587177 mE/ 4090945 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate)

APN: 007-331-04

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The one-story commercial building was built during 1958 in the Contemporary architectural style, with exterior walls clad in painted stucco or painted T1-11 plywood siding and the roof clad in composite shingles. The building is rectangular in plan, with a low-pitched front-gable roof, wide overhanging eaves, and exposed roof beams. The roof extends northward over a space open on the main (northeast) elevation, similar in design to a car port and currently used for storage. The enclosed part of the building is bilaterally symmetrical, with a central double-leaf full-lite entry door flanked on either side by a large rectangular fixed window and a smaller rectangular fixed window, both tilted forward, with a series of quadrilateral-shaped fixed windows in the gable end above (Figure 1). (see continuation sheet)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: ☒ Building

☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession #) Main elevation, looking SW. 01-31-2020.

IMG 0623

*P6. Date Constructed/Age and Source: ☒ Historic ☐ Prehistoric ☐ Both 1958 (architectural plans)

*P7. Owner and Address:

City of Santa Cruz

809 Center Street

Santa Cruz, CA 95060

*P8. Recorded by: (Name, affiliation, and address) Kara R. Dotter, Dudek

725 Front Street, Suite 400
Santa Cruz, CA 95060

*P9. Date Recorded: 02/05/2020

*P10. Survey Type: (Describe)

Pedestrian

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Dotter, Kara R., Fallin Steffen, and Kathryn Haley. 2020. Historic Resources Technical Report for 15 Municipal Wharf, Santa Cruz, California. Prepared for the City of Santa Cruz. Prepared by Dudek. February 2020.

*Attachments: ☐ NONE ☒ Location Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record

☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record

☐ Artifact Record ☐ Photograph Record ☐ Other (List): _____

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LOCATION MAP

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Page 2 of 15 *Resource Name or # (Assigned by recorder) 15 Municipal Wharf
*Map Name: Santa Cruz USGS 7.5' Topo *Scale: 1:24,000 *Date of map: 1954 (revised 1994)



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BUILDING, STRUCTURE, AND OBJECT RECORD

*Resource Name or # (Assigned by recorder) 15 Municipal Wharf *NRHP Status Code _____
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B1. Historic Name: _____
B2. Common Name: _____
B3. Original Use: Commercial - boat rentals B4. Present Use: Commercial - boat rentals
*B5. Architectural Style: Contemporary
*B6. Construction History: (Construction date, alterations, and date of alterations)
Built 1958. Enclosure of open carport-like area (c. 2016-2019. Two painted murals on main elevation (unknown date. Attachment of metal grid to round windows (unknown date). Removal and alterations of the architect's original interior design (unknown date). Extension of original 1914 pier to accommodate the existing building (c. 1957). Second extension of pier off the rear and northwest elevations (c. 1982).
*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: _____ Original Location: _____
*B8. Related Features: _____

B9a. Architect: _____ b. Builder: _____
*B10. Significance: Theme N/A Area N/A
Period of Significance N/A Property Type N/A Applicable Criteria N/A
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Development of the Santa Cruz Municipal Wharf (1914)

The completion of the most recent pier in Santa Cruz occurred in December 1914. The Santa Cruz Municipal Wharf represented a fusion between the shipping and tourism industries within Santa Cruz because "in contrast to the city's early wharves that had warehouses and businesses associated with fishing and shipping, the current wharf [was intended to be] tourist oriented with restaurants, stores and souvenir shops" (Lehman 2000: 27).

The following is a brief history of the wharf excerpted from a report by Bridgette Maley of architecture + history, LLC as part of the Santa Cruz Wharf Master Plan project (Maley 2016: 3-5):

The Santa Cruz Wharf was constructed in 1914, as the last in a series of six piers that were constructed on the Santa Cruz waterfront between 1849 [1847] and 1914. The Wharf was originally envisioned as (see continuation sheet)

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

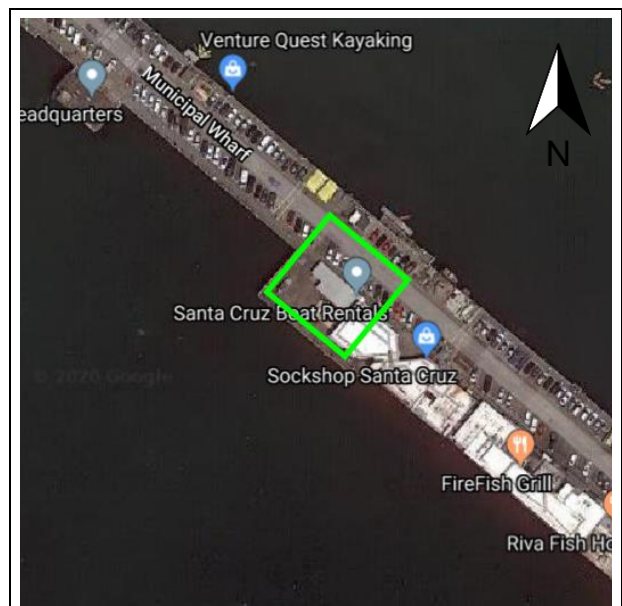
(see continuation sheet)

B13. Remarks:

*B14. Evaluator: Kara R. Dotter, MSHP

*Date of Evaluation: February 5, 2020

(This space reserved for official comments.)



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*P3a. Description: (continued)

Two decorative murals are painted onto the stucco cladding at each end of the main elevation, adjacent to the building's corners.

The northwest elevation presents as two sections (Figure 2). Approximately two-thirds of the width, representing the side of the carport-like area, comprises four vertical wood timbers supporting four roof beams oriented end-on that then support the shingle-clad roof. The bottom of the wall appears to be clad in stucco, while the upper half is open to the elements. Two of the open sections are covered with a grid of sturdy metal wires, and the third open section was recently covered with T1-11 plywood siding. The remaining third of the elevation is recessed approximately 10 feet and presents as a monolithic stucco-clad wall.

The southeast elevation also presents as two sections (Figure 3). The first section is recessed approximately 6 feet, representing the rear of the carport-like portion, and was recently clad with T1-11 plywood siding. The second section, corresponding to the enclosed portion of the building, displays bilateral symmetry, with two fixed round windows protected by a grid of sturdy metal wires and six quadrilateral-shaped fixed windows above on the gable end. The wall surface between the quadrilateral windows appears to be clad with painted horizontal wood boards. Wide overhanging eaves supported by exposed roof beams shelter the gable-end windows below.

The southeast elevation abuts the adjacent building, leaving only approximately three feet of stucco-clad wall exposed adjacent to the main elevation (Figure 4).

Observed Alterations:

- Enclosure of open carport-like area(c. 2016-2019)
- Removal of eaves along southeast elevation and construction of new building against exterior wall; subject property no longer free-standing (c. 1968-1982)
- Two painted murals on main elevation (unknown date)
- Attachment of metal grid to round windows (unknown date)
- Removal and alterations of the architect's original interior design
- Extension of original 1914 pier to accommodate the existing building (c. 1957)
- Second extension of pier off the rear and northwest elevations (c. 1982)

*B10. Significance: (continued)

a commercial enterprise, built with public funds to further the economic development of the City. Soon after its construction, the Wharf became an attractive facility for the mooring and off-loading of commercial fishing vessels.

Since its construction in 1914, the Wharf has expanded from approximately and widened from its original 100-foot width over the years, but the location and length of the Wharf remain unchanged. Since its construction in 1914, the Wharf has expanded from approximately 4.2 acres to 7.5 acres. The Wharf increased by 3.3 acres between the 1950s and the 1980s for commercial uses and parking.

Davits for lifting fishing vessels into and out of the ocean once lined the Municipal Wharf, as did a rail line, warehouses, and fishing-related storefronts. The rail line was taken out in the late 1940s to early 1950s,

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and replaced with a paved deck for vehicle traffic.

Over the years, the Wharf has evolved in role, function and identity. From its initial role as a cargo handling and shipping pier to its later adaptation to serve the commercial fishing industry, the early decades of the Wharf were closely tied to the resources of Monterey Bay. After World War II and beginning in the 1950s, the Wharf was significantly expanded for commercial uses and parking. The commercial uses were initially a direct outgrowth of the commercial fishing industry, incorporating fish sales and featuring prepared seafood dishes in an open air setting in close conjunction with off-loading and handling of the daily catch.

Many of the Wharf's original buildings and structures have been demolished or greatly altered, including a large warehouse building that was located at the bayward end of the Wharf. According to the Wharf Master Plan, this structure is significant from a historic point of view because "it gave physical expression to the environmental conditions that made deepwater maritime functions possible and that contributed to the configuration of the end of the Wharf to optimize berthing relative to wind and wave conditions." Today, the largely industrial and commercial oriented historic-era businesses of the Municipal Wharf have been replaced by restaurants, retail, and other tourist oriented services. Over 20 buildings and structures associated with these businesses are located on the deck of the Wharf, as well as other built environment features associated with utilities and lighting.

The Historic Context Statement for the City of Santa Cruz (Lehmann, 2000) indicates that within the context Santa Cruz's economic development between 1850 and 1950, the Municipal Wharf represents an important property type related to industrial development and transportation. The wharves and piers first built in the early days of Santa Cruz's history evolved with the changes in the City's economy. The shipping wharves gave way to a railroad wharf and finally to a fishing and commercial wharf that was constructed in 1914 to serve the City's needs. The Wharf has been rebuilt and refurbished over time and the warehouses and commercial fishing boats have been replaced by restaurants and concessions for sports fishing and sightseeing. The circa 1920s fishing boat, the Marcella, is on display at the north wharf, and, as stated by Lehmann, may be the "last remnant of the time when the waterfront served the needs of a booming industrial economy."

As the last of a series of six piers constructed to serve industrial and commercial development in Santa Cruz, and which have been a defining element of the Santa Cruz waterfront since 1849, the Santa Cruz Municipal Wharf serves as a vital physical reminder of that history. The historical fishing industry was also heavily dependent on the wharves of Santa Cruz, including the Municipal Wharf, prior to 1950.

The Wharf no longer serves the commercial fishing industry. Currently, the Wharf is one of a number of destination attractions in Santa Cruz's Beach Area. The City estimates that approximately 2.5 million visitors currently come to the Wharf annually. Although the Wharf provides opportunities for pier fishing, as well as kayak and small fishing boat rentals, the Wharf's identity is primarily related to the commercial uses along its length.

Measuring 2,745 feet in total length, the Wharf presently constitutes the longest example of a wooden pier structure in California. It is also the longest pier of any kind located

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on the Pacific Ocean coastline (See California 2020).

Maley also describes the physical construction of the wharf in the context of other, similar structures (Maley 2016: 3):

The Wharf is a timber pier, entirely constructed of wood and supported by approximately 4,445 Douglas fir piles. The Wharf is the longest timber piles supported pier structure in the United States and one of the longest in the world. Called a wharf because of its early function in off-loading cargo, the Santa Cruz Wharf is actually a pier structure that extends to deep water, historically facilitating the mooring of large vessels, unlike a wharf which typically runs parallel to the shore.

Specifically, the Wharf piles are Douglas fir measuring 16 inches in diameter. The piles are along driven rows, or "bents," spaced on-center at approximately 15 feet apart. The piles and bents are spanned by 6" x 12" and 4" x 12" beams, also known as "stringers." The piles are each numbered, starting from the farthest point and going west to the shore. The Wharf deck features two inches of asphalt on walkways and roads (Maley 2016: 3; Pers.Comm. Bombaci).

After initial construction in 1914, the wharf underwent a series of additions and alterations. Additions to the narrow footprint of the wharf occurred on both sides during construction campaigns in the 1950s, during 1968-1972, during 1970-1980s, in 1982, and in 2008 (Figure 5).

The subject property was designed by the City of Santa Cruz Department of Engineering in early 1958, during the 1950s period of expansion to the wharf. Constructed on a purpose-built addition to the original pier, the building provided a new, larger facility for the boat rental operations, which were originally located directly opposite the subject property.

Architectural Style: Contemporary

The subject property was designed in the Contemporary architectural style, which was popular c. 1945-1990 and dominated avant-garde domestic architecture in the 1960s. Contemporary buildings drew inspiration from Frank Lloyd Wright's Usonian houses and his writing in shelter magazines and professional journals. Character-defining features of the Contemporary style include:

- Low-pitched gable roofs with wind overhanging eaves
- Exposed roof beams
- Windows in gable ends or just below the roofline on non-gabled elevations
- Extensive use of natural materials, including wood, stone, brick, and occasionally concrete
- Broad expanses of uninterrupted wall surfaces, especially on the main elevation
- Asymmetrical shape
- Typically with a recessed or obscured entry door
- Front-gabled subtypes include a broad one-story form with a very low-pitched roof that was popularized by Joseph Eichler (1900-1974), a California developer

Significance Evaluation

The Santa Cruz Wharf was found significant for listing in the CRHR under Criterion 1, and under Santa Cruz Criteria. The Santa Cruz Wharf also appears eligible for listing in the

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NRHP under Criterion A for its continued associations with the industrial development, transportation, commerce, and recreation development of the City. Additionally, it appears eligible under NRHP Criterion C as a distinctive engineering structure known to be the longest wooden pier structure located along the United States Coast of the Pacific Ocean (Dudek 2019). The character-defining features associated with the Santa Cruz Wharf are its location, setting, alignment, wooden materials (piers), its original length of 2,745 feet long and its continued function as a wharf structure. The contemporary infrastructural elements on the wharf deck, including roads, lighting, utilities, and buildings, do not date to the 1914 period of significance identified for the Santa Cruz Wharf. As such, they are considered non-contributing elements of the structure.

As noted above, the buildings located on the Wharf are not contributing elements of the historical resource. The purpose of the following evaluation is to assess the property's historical significance and integrity as an individual property. The building located at 15 Municipal Wharf was recorded and evaluated in consideration of NRHP, CRHR, and City of Santa Cruz designation criteria and integrity requirements. The property significance evaluation was prepared by Dudek architectural historian Kara R. Dotter, MSHP, who meets the Secretary of the Interior's Professional Qualification Standards for architectural history.

NRHP/CRHR Statement of Significance

Criterion A/1: That are associated with events that have made a significant contribution to the broad patterns of our history.

The subject property is a late addition to the Santa Cruz Municipal Wharf, providing a new building to house the boat rental operation that was originally located on the opposite side of the wharf. The Santa Cruz Municipal Wharf itself is significant as the last built representative of the important function that shipping played in the early economic growth and the sustained development of the City of Santa Cruz. However, the subject property merely rests upon the wharf structure. Additionally, it is not the original home of the boat rental operation, nor does the function it serves associate it with the shipping industry and the part it played in the early economic growth and sustained development of the City of Santa Cruz. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion A/1.

Criterion B/2: That are associated with the lives of persons significant in our past.

To be found eligible under B/2 the property has to be directly tied to an important person and the place where that individual conducted or produced the work for which he or she is known. Archival research failed to indicate any direct association of 15 Municipal Wharf with individuals known to be historic figures at the national, state, or local level. As such, the subject property is not known to have any historical associations with people important to the nation's or state's past. Due to a lack of identified significant associations with important persons in history, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion B/2.

Criterion C/3: That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction.

The subject property is a modest, unremarkable representation of the Contemporary architectural style. It retains several features indicative of the Contemporary architectural style, including low-pitched gable roofs with wide overhanging eaves, exposed roof beams, windows in gable ends or just below the roofline on non-gabled elevations, extensive use of natural materials, broad expanses of uninterrupted wall surfaces, an overall asymmetrical shape, and as a front-gabled subtype it includes a broad one-story form with a very low-pitched roof popularized by Joseph Eichler (1900-

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1974), a California developer. Despite retention of such design features, the building has underwent several alterations, including enclosure of the open carport-like area; removal of eaves along southeast elevation and construction of a new building against the exterior wall, meaning the subject property is no longer free-standing; two painted murals on main elevation; attachment of metal grid to round windows; and removal of and alterations to the architect's original interior design. Additionally, the building does not represent the work of a master, it does not possess high artistic values, nor does it represent a significant and distinguishable entity whose components may lack individual distinction. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion C/3.

Criterion D/4: That have yielded, or may be likely to yield, information important in prehistory or history.

There is no evidence to suggest that the subject property has the potential to yield information important to state or local history. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under NRHP/CRHR Criterion D/4.

City of Santa Cruz Statement of Significance

City of Santa Cruz Criteria 1-6 are similar to the four criteria for listing on the NRHP or CRHR. As such, for all the reasons discussed above, 15 Municipal Wharf is recommended as not eligible for listing under City of Santa Cruz Criteria 1-6. Regarding Criteria 7, the numerous additions to and alterations of the subject property as originally designed diminish its integrity of location, design, workmanship, materials, and feeling. Integrity of setting is lost due to the alteration from its original condition as a free-standing building with limited access on the west and south elevations. Integrity of association remains, as it still serves its original function as a boat rental facility, retaining its ties to the tourism industry centered on the wharf. As such, the subject property does not retain sufficient integrity to accurately convey its significance. Therefore, 15 Municipal Wharf is recommended as not eligible for listing under any City of Santa Cruz criteria.

Integrity Discussion

In addition to meeting one or more of the above criteria, an eligible resource must retain integrity, which is expressed in seven aspects: location, design, setting, workmanship, materials, feeling, and association. All properties change over the course of time. Consequently, it is not necessary for a property to retain all its historic physical features or characteristics. The property must retain, however, the essential physical features that enable it to convey its historic identity. The essential physical features are those features that define both why a property is significant and when it was significant.

In the case of 15 Municipal Wharf, the numerous additions to and alterations of the subject property as originally designed diminish its integrity of location, design, workmanship, materials, and feeling. Integrity of setting is lost due to the alteration from its original condition as a free-standing building with limited access on the west and south elevations. Integrity of association remains, as it still serves its original function as a boat rental facility, retaining its ties to the tourism industry centered on the wharf. As such, the subject property does not retain sufficient integrity to accurately convey its significance.

Summary of Evaluation Findings

As a result of research, documentation, and evaluation of the subject property, 15 Municipal Wharf is recommended as not eligible for listing under any NRHP, CRHR, or City of Santa Cruz designation criteria. Additionally, the subject property does not retain sufficient integrity for listing. The subject property was evaluated in accordance with

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Section 15064.5(a) (2)-(3) of the CEQA Guidelines using the criteria outlined in Section 5024.1 of the California Resources Code, and for the reasons detailed above it does not constitute a historical resource for the purposes of CEQA.

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Figures



Figure 1. Main (northeast) elevation, view looking southwest (IMG_0634)

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Figure 2. Northwest elevation, view looking southeast (IMG_0595)

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Figure 3. Rear (southwest) elevation, view looking east (IMG_0598)

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Figure 4. Southeast elevation, view looking west (IMG_0619)

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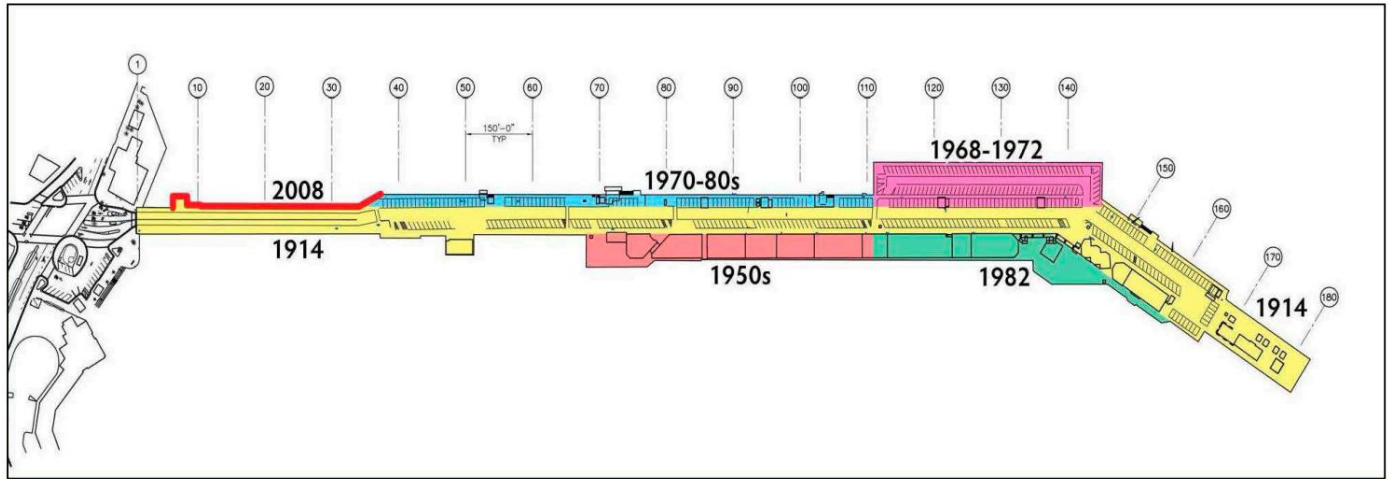


Figure 5. Graphic representation Santa Cruz Municipal Wharf's development over time.

